



The Corporation of the
Town of Milton
Committee of Adjustment and Consent

Thursday, June 25, 2026, 6:00 p.m.
Council Chambers - In Person

Applicants and interested parties can participate in person at Town Hall, Council Chambers, 150 Mary Street.

	Pages
1. AGENDA ANNOUNCEMENTS / AMENDMENTS	
2. DISCLOSURE OF PECUNIARY INTEREST	
3. REQUESTS FOR DEFERRAL	
3.1 A26-029/M 387 Campbell Avenue East Application A26-029/M for 387 Campbell Avenue East will be deferred until the July 30th Committee of Adjustment Hearing.	
4. MINUTES	
4.1 Minutes of Committee of Adjustment Hearing held on May 28, 2026	
5. HOUSEKEEPING	
6. ITEMS FOR CONSIDERATION	
6.1 A25-049/M 8600 Escarpment Way The applicant is seeking to address existing lot coverage permissions solely on 8600 Escarpment Way to alleviate the need, in future, to seek an application under Section 45(2) of the Planning Act for an expansion/extension of a legal non-conforming use. Secondly, the applicant is seeking relief to accommodate a shared driveway connection between the 8600 Escarpment Way and 0 Escarpment Way.	3
6.2 A25-050/M 0 Escarpment Way The applicant is seeking relief to address the deficiencies that are a result of the existing operation at 8600 Escarpment Way expanding onto 0 Escarpment Way.	8
6.3 A26-025/M 1190 Bronte Street South The applicant is seeking to facilitate a commercial plaza that includes nine buildings with vehicular access from Louis St. Laurent Avenue and Bronte Street South.	15

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|-----|---|----|
| 6.4 | A26-027/M 11340 Taylor Court
The applicant is seeking to construct a second attached garage, projecting from the eastern side of the dwelling, with a storage loft above. As the Zoning By-law only permits one garage per lot in the applicable zone, a minor variance is required. | 22 |
| 6.5 | A26-030/M 387 Pine Street
The applicant is seeking to renovate the existing dwelling and construct a two-storey addition. To facilitate the proposed design, minor variances are required for additional lot coverage and a reduced garage setback. In addition, the application seeks to legalize the total GFA of two existing accessory structures. | 35 |
| 6.6 | A26-031/M 171 Bronte Street
The applicant is seeking to convert the accessory structure into an additional residential unit (ARU). To facilitate the proposal, a minor variance is needed to legalize the setback of the existing deck attached to the accessory structure. | 45 |
| 6.7 | A26-026/M 280 Sydney Street
The applicant is seeking to convert the existing detached garage into an Additional Residential Unit (ARU) and to utilize the existing site conditions. | 54 |
| 6.8 | A26-028/M 10725 Louis St.Laurent
The applicant is seeking to obtain relief from the maximum floor area permitted for two detached accessory structure for waste storage as well as allow for an increase in height for both structures. | 61 |
| 7. | NEXT MEETING
Thursday, July 30, 2026, commencing at 6:00 p.m. | |
| 8. | ADJOURNMENT | |



The Corporation of the Town of Milton

Report To: Committee of Adjustment and Consent

From: Rachel Suffern, MPA, M.Sc. (PI), MCIP, RPP

Date: June 25, 2026

File No: A25-049M

Subject: 8600 Escarpment Way

Recommendation: That the application for minor variance be approved, subject to the following condition forming the variance for a reduced setback from a parking area from a lot line:

1. The variance only applies as long as 8600 and 0 Escarpment Way are in common ownership.

No conditions are associated with relief being granted for the Lot Coverage permission under Section 8.2 Table 8B - Footnote (*3).

General Description of Application

Under Section 45(1) of the Planning Act, the following minor variance to Zoning By-law 016-2014, as amended, has been requested to allow:

- A Transportation Terminal, operating at 8600 Escarpment Way, to have a Lot Coverage of 16% whereas Section 8.2 Table 8B: Footnote (*3) of Comprehensive Zoning By-law 016-2014, as amended, requires a minimum Lot Coverage of 25%; and,
- Allow a parking area setback of 0.0 metres from a lot line, whereas the Zoning By-law requires 1.5 meters (a reduction of 1.5 metres).

The Subject Property is known municipally as 8600 Escarpment Way. The property is located north of James Snow Parkway on the west side of Escarpment Way. 8600 Escarpment Way is currently operating as a legally established Transportation Terminal and has two buildings, with a collective footprint of approximately 6,892 square metres, that support the principal use by way of administrative, maintenance and logistical support. Surrounding land uses generally include light industrial with service commercial to the east (fronting onto Regional Road 25).

Firstly, the applicant is seeking to address existing lot coverage permissions solely on 8600 Escarpment Way to alleviate the need, in future, to seek an application under Section 45(2) of the Planning Act for an expansion/extension of a legal non-conforming use. While the Transportation Terminal use is permitted as-of-right on the site, the operation was established prior to the existing lot coverage requirements being in place; as such, the existing lot coverage of 16%, while legally protected as a legal non-complying condition, would trigger the need for an application in-future,



General Description of Application

unless the proponent seeks a minor variance for the lot coverage now which would alleviate the legal non-conforming status.

Secondly, the applicant, being the owner of the Subject Property and the abutting parcel (0 Escarpment Way) intends to expand the existing transportation terminal use operating at 8600 Escarpment Way onto 0 Escarpment Way. As such, the applicant is seeking relief to accommodate a shared driveway connection between the two sites.

Official Plan Designation (including any applicable Secondary Plan designations)

The Subject Property is designated as Industrial Area on Schedule B - Urban Land Use Plan within the Town of Milton Official Plan ('Official Plan'). The lands are further designated as Industrial Area on Schedule C.2.B - Milton 401 Industrial Business Park Secondary Plan Land Use Plan ('Secondary Plan'). Within the Official Plan and Secondary Plan, a Transportation Terminal is a permitted use.

Based on the aforementioned and in consideration of the proposed conditions of approval, Planning Staff is of the opinion that the proposal is in conformity with the Town of Milton Official Plan and Secondary Plan.

Zoning

The subject lands are zoned as General Industrial (M2) within the Town of Milton Urban Zoning By-law 016-2014, as amended. The M2 Zone permits a range of general industrial uses such as manufacturing and Transportation Terminals.

Variance No. 1 - Minimum Lot Coverage

Section 8.2 Table 8B: Footnote (*3) of the Zoning By-law requires that a lot containing a Transportation Terminal have a minimum lot coverage of 25%. To accommodate the applicant's proposal to recognize the existing Lot Coverage at 8600 Escarpment Way, a reduced lot coverage of 16% is being requested.

The intent of the above-noted lot coverage requirement is to ensure that a Transportation Terminal has a significant built form component on site and gross floor area that provides administrative, maintenance and logistical support to the principal use. For clarity, standalone commercial motor vehicle parking is not a permitted use within the Zoning By-law.

Variance No. 2 - Minimum Setback to Parking Areas

Section 5.2 Table 5L of the Zoning By-law requires that parking areas in an employment zone to be setback a minimum of 1.5 metres from interior lot lines. To facilitate a shared access between both 0 and 8600 Escarpment Way (located at their shared interior lot line), a reduced parking area setback of 0.0 metres is required.



Zoning

The intent of this provision is to ensure that parking areas are adequately setback from shared lot lines in order to mitigate any impacts on abutting parcels (and their operations), along with protecting for adequate permeable surface that will accommodate a site's run-off.

With the exception of the above-noted provisions for which relief is being sought, the proposal complies with all other provisions of the Zoning By-law.

Consultation

Public Consultation

Notice for the hearing was provided pursuant to the Planning Act. As of the writing of this report on, June 17, 2026, Planning Staff has not received any comments from members of the public.

Agency Consultation

No objections were filed with respect to the variance application from Town Staff or external agencies.

Development Services Comments

Variance No. 1 - Minimum Lot Coverage

The applicant intends to continue the existing Transportation Terminal on the Subject Property. Given the use was legally established prior to the existing Zoning By-law 016-2014, as amended, taking effect, the existing Transportation Terminal provisions pertaining to the lot coverage requirement didn't apply at the time of establishment. As such, while the Subject Property legally has non-conforming status as it relates to lot coverage, the applicant is seeking to recognize the 16% lot coverage through this application so that in future, should any changes be made strictly to 8600 Escarpment Way, an application under Section 45(2) of the Planning Act (i.e. an expansion/extension to a legal non-conforming use) would not be required.

The applicant has requested to reduce the required lot coverage associated with a Transportation Terminal - the Zoning By-law requires a minimum lot coverage of 25%, where as the applicant is proposing 16% (a reduction of 9%).

Planning Staff do not have a concern with the reduction in lot coverage on the basis that the current operation has demonstrated that the existing building is adequate in size for the associated functions in that it supports the overall operation (i.e. dispatch, administration, and maintenance). Further, the existing buildings are significant in that they create a substantial built-form presence on site and anchors the development from a street façade perspective.

Variance No. 2 - Minimum Setback to Parking Areas

To facilitate the proposal whereby the existing Transportation Terminal operation at 8600 Escarpment Way is being expanded onto the abutting parcel, 0 Escarpment Way, the Subject Property requires relief as it relates to a minimum setback to a parking area. By creating a shared internal access between 0 Escarpment Way and 8600 Escarpment Way, a deficiency is being created whereby there is a 0.0 metre setback between a lot line and a parking area. The Zoning



Consultation

By-law does not differentiate between a parking space and a drive aisle - therefore, by virtue of an internal drive aisle being located at the shared interior lot line, the required setback cannot be provided.

Planning Staff is of the opinion that the reduced setback is minor in nature and is desirable in this instance because it provides shared access between the properties which supports the cohesive vision of the lands so far as a single Transportation Terminal operating across both. Further, the reduced setback does not extend the entirety of the shared lot lines and ample permeable surface and buffering is provided.

Planning Staff does advise that shared access is contingent on both parcels remaining in common ownership; should either parcel be conveyed to different ownership, a consent application would then be required to provide for a reciprocal access easement.

Based on the review of this application and the inclusion of the recommended conditions of approval, Planning Staff offers no objection to the approval of this application. The proposal is in conformity with the Official Plan, maintains the intent of the Zoning By-law, is desirable for the development of the subject lands, and is minor in nature.

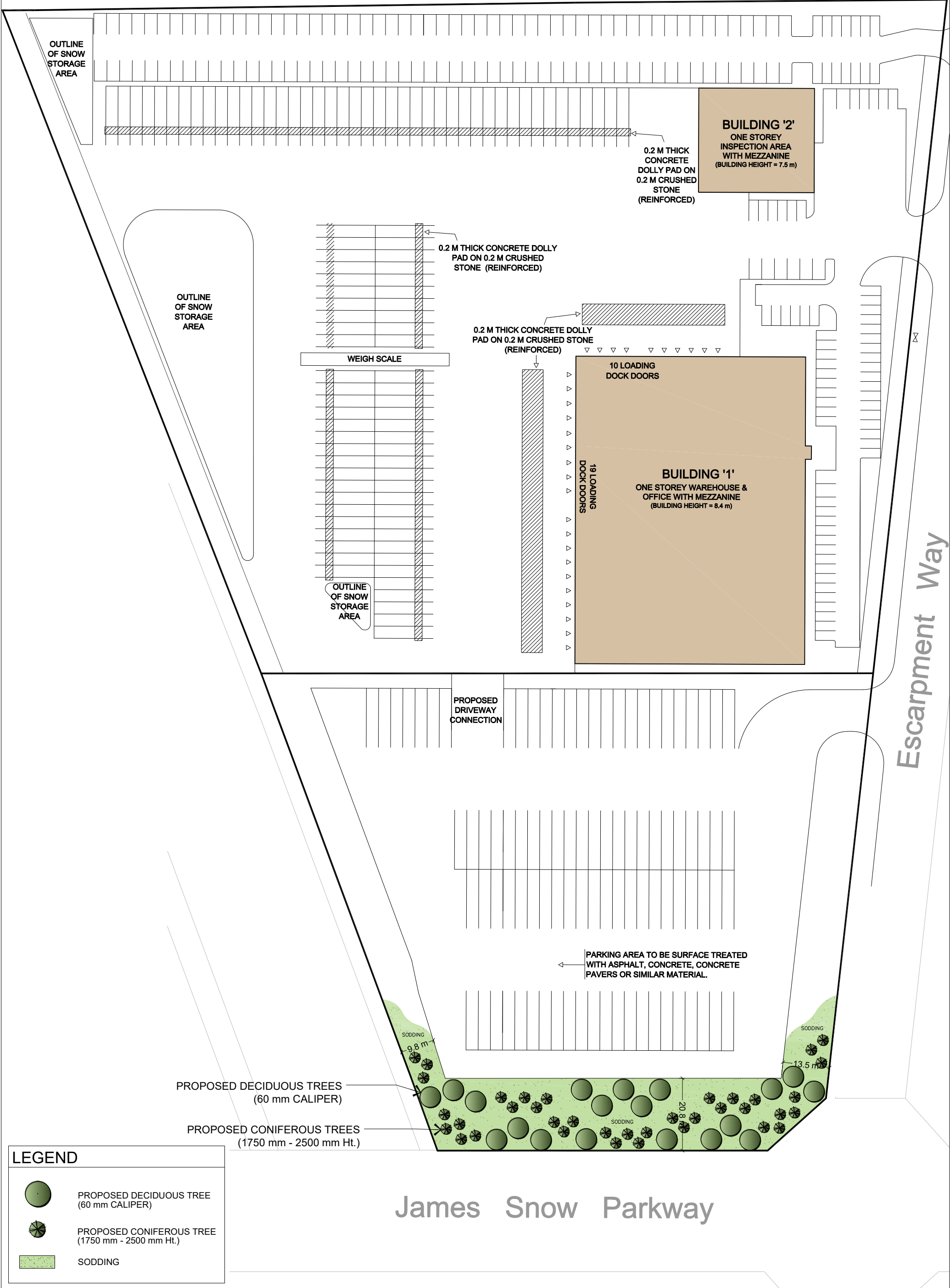
Respectfully submitted,

Rachel Suffern, MPA, M.Sc. (PI), MCIP, RPP
Senior Planner, Development Review

For questions, please contact: Rachel.Suffern@Milton.ca Phone: Ext. 2263

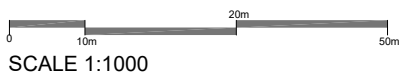
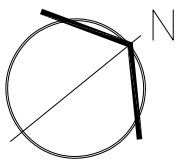
Attachments

Figure 1 - Site Plan



CONCEPTUAL LANDSCAPE BUFFER PLAN
8600 ESCARPMENT WAY
MILTON, ONTARIO

PROJECT: 6223
DATE: 13 MAY, 2026





The Corporation of the Town of Milton

Report To: Committee of Adjustment and Consent

From: Rachel Suffern, MPA, M.Sc., MCIP, RPP

Date: June 25, 2026

File No: A25-050/M

Subject: 0 Escarpment Way

Recommendation: That the application for minor variance be approved, subject to the following conditions which shall form of the variance:

1. The variance only applies as long as 8600 and 0 Escarpment Way are in common ownership;
2. An Entrance Permit be obtained for the site and any non-approved work within the Town boulevard be restored; and,
3. The parking area, landscaping, and site design be constructed/established in accordance with site plan drawing Strybos King, dated May 13, 2026.

General Description of Application

Under Section 45(1) of the Planning Act, the following minor variances to Zoning By-law 016-2014, as amended, has been requested to:

1. Amend the definition of a 'Transportation Terminal' in respect of 0 Escarpment Way to read as: "[meaning] a premises where trucks, trailers, or transports are dispatched for hire as common carriers or where freight handling facilities, such as pick-up, delivery and transitory storage of goods incidental to motor freight shipment is provided and where the use operates in conjunction with a building, structure, pr part thereof located at 8600 Escarpment Way.";
2. Allow a minimum lot coverage of 0%, whereas the Zoning By-law requires a minimum of 25% for a Transportation Terminal use (a reduction of 25%); and,
3. Allow a parking area setback of 0.0 metres from a lot line, whereas the Zoning By-law requires 1.5 meters (a reduction of 1.5 metres).

The Subject Property is municipally known as 0 Escarpment Way and legally described as Block 16 on 20M-952. The property is located immediately north of James Snow Parkway on the west side of Escarpment Way. 0 Escarpment Way is currently vacant with no legal land use established. Surrounding land uses include an existing Transportation Terminal to the north, light industrial to the west, and service commercial to the east (fronting onto Regional Road 25).

Both the Subject Property and the adjacent parcel, 8600 Escarpment Way, are under common ownership (1912900 Ontario Inc. and Escarpment Way Properties Inc.). 8600 Escarpment Way is

General Description of Application

currently operating as a legally established Transportation Terminal and has two buildings, with a collective footprint of approximately 6,892 square metres, that support the principal use by way of administrative, maintenance and logistical support. The owner intends to expand the existing Transportation Terminal use operating at 8600 Escarpment Way onto 0 Escarpment Way. The owner does not require additional space for administrative/logistical support and therefore intends to use the additional lot area located at 0 Escarpment Way for surplus parking and site circulation.

Despite common ownership, 0 and 8600 Escarpment Way remain separately conveyable parcels and therefore, each requires a separate application (for minor variances related to 8600 Escarpment Way, see Report A25-049/M). However, Planning Staff emphasize that the minor variances requested through this application are only supportable on the basis of common ownership and an integrated operation. It is not the intent of this minor variance to allow 0 Escarpment Way to operate independently of 8600 Escarpment Way, thus if one, or both, properties are conveyed separately, the proposed minor variances no longer apply.

Official Plan Designation (including any applicable Secondary Plan designations)

The Subject Property is designated as Industrial Area on Schedule B - Urban Land Use Plan within the Town of Milton Official Plan ('Official Plan'). The lands are further designated as Industrial Area on Schedule C.2.B - Milton 401 Industrial Business Park Secondary Plan Land Use Plan ('Secondary Plan'). Within the Official Plan and Secondary Plan, a Transportation Terminal is a permitted use.

On Schedule C.2.A - Milton 401 Industrial Business Park Secondary Plan Structure Plan, James Snow Parkway is identified as a 'Gateway Street'. As set out in Section C.2.2.2.1 of the Secondary Plan, the identification of a Gateway Street signals that the corridor is a key gateway to the town and requires enhanced streetscape design and treatment, along with a higher order of development on lots abutting such right-of-ways. The policies in Section C.2.4.5.8 require that lots abutting key gateways shall have screened parking by way of berming and landscaping.

The applicant has provided a Landscape Plan that shows enhanced landscaping and screening along the James Snow Parkway frontage - Staff is of the opinion that the proposed works will satisfy the Official Plan and Secondary Plan policies and allow Escarpment Way and 8600 Escarpment Way to function as a fully integrated whole.

Based on the aforementioned and in consideration of the proposed conditions of approval, Planning Staff is of the opinion that the proposal is in conformity with the Town of Milton Official Plan and Secondary Plan.

Zoning

The subject lands are zoned as General Industrial (M2) within the Town of Milton Urban Zoning By-law 016-2014, as amended. The M2 Zone permits a range of general industrial uses such as manufacturing and Transportation Terminals.

Variance No. 1 - Definition of Transportation Terminal

Zoning

Section 3 of the Zoning By-law 016-2014, as amended, defines a Transportation Terminal as:

“[meaning] a building, structure, or part thereof where trucks, trailers, or transports are dispatched for hire as common carriers or where freight handling facilities, such as pick-up, delivery and transitory storage of goods incidental to motor freight shipment is provided and may include an office building accessory to the transportation terminal use and facilities for the service and repair and washing of the trucks, trailers or transports.”

The intent of the definition of a Transportation Terminal is to ensure that an operation offers all logistical and ancillary needs associated with the movement of goods. Further, the definition seeks requires a built-form component and does not allow standalone commercial motor vehicle parking. In other words, a Transportation Terminal, as defined, is not a truck parking lot.

To facilitate the proposal, and allow the owner to expand the existing Transportation Terminal use at 8600 Escarpment Way onto 0 Escarpment Way, the definition of Transportation Terminal in respect to 0 Escarpment Way (only) is proposed to be amended to the following:

“mean a premises where trucks, trails, or transports are dispatched for hire as common carriers or where freight handling facilities, such as pick-up, delivery and transitory storage of goods incidental to motor freight shipment is provided and where the use operates in conjunction with a building, structure, or part thereof located at 8600 Escarpment Way.”

Staff advise that the definition of a Transportation Terminal is not being amended for 8600 Escarpment Way, which still requires a building or structure. Given the presence of Transportation Terminal buildings on 8600 Escarpment Way, it is Staff’s opinion that additional buildings are not also required on 0 Escarpment Way so long as the operation of the two properties is integrated and under common ownership.

Variance No. 2 - Minimum Lot Coverage

Section 8.2 Table 8B: Footnote (*3) of the Zoning By-law requires that a lot containing a Transportation Terminal have a minimum lot coverage of 25%. To accommodate the applicant’s proposal whereby two abutting lots, under common ownership, will be used collectively to operate (and expand) an existing Transportation Terminal already established at 8600 Escarpment Way, a reduced lot coverage of 0% is being requested for 0 Escarpment Way.

The intent of the above-noted lot coverage requirement is to ensure that a Transportation Terminal has a significant built form component on site and gross floor area that provides administrative, maintenance and logistical support to the principal use. For clarity, standalone commercial motor vehicle parking is not a permitted use within the Zoning By-law.

Variance No. 3 - Minimum Setback to Parking Areas

Section 5.2 Table 5L of the Zoning By-law requires that parking areas in an employment zone to be setback a minimum of 1.5 metres from interior lot lines. To facilitate a shared access between both 0 and 8600 Escarpment Way (located at their shared interior lot line), a reduced parking area setback of 0.0 metres is required.



Zoning

The intent of this provision is to ensure that parking areas are adequately setback from shared lot lines in order to mitigate any impacts on abutting parcels (and their operations), along with protecting for adequate permeable surface that will accommodate a site's run-off.

With the exception of the above-noted provisions for which relief is being sought, the proposal complies with all other provisions of the Zoning By-law.

Consultation

Public Consultation

Notice for the hearing was provided pursuant to the Planning Act on June 15, 2026. As of the writing of this report on, June 17, 2026, Planning Staff has not received any comments from members of the public.

Agency Consultation

No objections were filed with respect to the variance application from Town staff or external agencies.

Development Engineering has requested that the applicant must establish a legal site entrance to the satisfaction of the Town of Milton, in accordance with the Town's current standards and specifications. Any work that has previously been completed must be restored to Town standards. Planning Staff has included a condition to fulfill this work as a condition of approval, via an Entrance Permit, should this application be approved.

Staff also advise that a Site Alteration Permit (in accordance with By-law 094-2022) would be required to hardscape the Subject Property which is a requirement of the Zoning By-law.

Development Services Comments

The applicant intends to expand the established Transportation Terminal located at 8600 Escarpment Way onto 0 Escarpment Way. Given both properties are separate blocks on a registered plan, they cannot be merged and remain separately conveyable - in this regard, the lands cannot be considered one lot for the purposes of zoning. Therefore, the relief set-out previously in this Report addresses deficiencies that are a result of the existing operation at 8600 Escarpment Way expanding onto the Subject Property.

Variance No. 1 - Definition of Transportation Terminal

To allow the existing Transportation Terminal operation on 8600 Escarpment Way to expand onto the Subject Property (a parcel reviewed independently under the Zoning By-law), the definition of a Transportation Terminal must be amended given there is no built form on the Subject Property and the operation would rely on the buildings located at 8600 Escarpment Way, which are under common ownership.

Amending the definition is technical in nature given the lots must be reviewed separately under the Zoning By-law. Further, the proposed language ensures that the use - and associated performance standards, including a reduced lot coverage (see Variance No. 2) - is directly



Consultation

associated with the existing operation at 8600 Escarpment Way. Planning Staff is of the opinion that the relief required in this instance is minor in nature as the existing Transportation Terminal will operate across both properties. For greater clarity, Planning Staff is only supporting the amended definition on the basis that it allows both properties to function collectively in support of one integrated, principal land use; otherwise, Planning Staff would not be supportive of amending the definition to alleviate the built form requirement (which would effectively be standalone commercial motor vehicle parking).

Variance No. 2 - Minimum Lot Coverage

To provide additional lot area for the abutting Transportation Terminal at 8600 Escarpment Way, the applicant has requested 0% lot coverage on 0 Escarpment Way. While a single operation is proposed across both properties, the Zoning By-law is to be applied independently against both lots and therefore, technically a minimum lot coverage of 25% would be required for each property. For context, if the lot coverage, yielding from the existing buildings located at 8600 Escarpment Way, was calculated based on the combined lot area of both properties, a lot coverage of 10.9% would be provided.

Planning Staff do not have a concern with the reduction in lot coverage on the basis that the current operation has demonstrated that the existing buildings are adequate in size to accommodate the ancillary functions that support the principal use (i.e. dispatch, administration, and maintenance). While both lots remain separately conveyable and subject to independent review through the Zoning By-law, the lots are proposed to function together. The added lot area (belonging to 0 Escarpment Way) will provide additional parking area, along with additional space to optimize site circulation. Further, the existing buildings are significant in that they create a substantial built-form presence on site and anchors the development from a street façade perspective. It is Planning Staff's opinion that the intent of the Zoning By-law is being maintained in this regard and further, that the development is desirable for the development of the subject lands as it is continuing an employment use within the Town's Employment Area while providing enhanced landscaping along James Snow Parkway that satisfies the applicable Official Plan (and Secondary Plan) policies.

To ensure the intent of minor variance - and Planning Staff's professional recommendation - is maintained, the following condition is proposed as part of the approval: "The variance only applies as long as 8600 and 0 Escarpment Way are in common ownership."

Should this application be approved, the applicant is advised that if the lots are transferred into separate ownership in future, the relief would no longer be applicable, and permissions granted through this application would be nullified. As noted above, the application's ability to meet the four-tests - including the intent of the Zoning By-law - relies heavily on the premise that the sites function together in support of a single Transportation Terminal operation. Should the existing Transportation Terminal operation and/or ownership change, 0 Escarpment Way would no longer have permission for a reduced lot coverage and any use on the Subject Property would require separate municipal approvals.

Variance No. 3 - Minimum Setback to Parking Areas



Consultation

By creating a shared internal access between 0 Escarpment Way and 8600 Escarpment Way, a deficiency is being created whereby there is a 0.0 metre setback between a lot line and a parking area. The Zoning By-law does not differentiate between a parking space and a drive aisle - therefore, by virtue of an internal drive aisle being located at the shared interior lot line, the required setback cannot be provided.

Planning Staff is of the opinion that the reduced setback is minor in nature and is desirable in this instance because it provides shared access between the properties which supports the cohesive vision of the lands so far as a single Transportation Terminal operating across both. Further, the reduced setback does not extend the entirety of the shared lot lines and ample permeable surface and buffering is provided.

Planning Staff does advise that shared access is contingent on both parcels remaining in common ownership; should either parcel be conveyed to different ownership, a consent application would then be required to provide for a reciprocal access easement.

Based on the review of this application and the inclusion of the recommended conditions of approval, Planning Staff offers no objection to the approval of this application. Subject to the fulfillment and ongoing compliance with the proposed conditions, the proposal is in conformity with the Official Plan, maintains the intent of the Zoning By-law, is desirable for the development of the subject lands, and is minor in nature.

Note: Additional permits are required under Town by-laws to implement the proposed development on 0 Escarpment way, including entrance and site alteration permits. A minor variance does not remove the requirements for these permits.

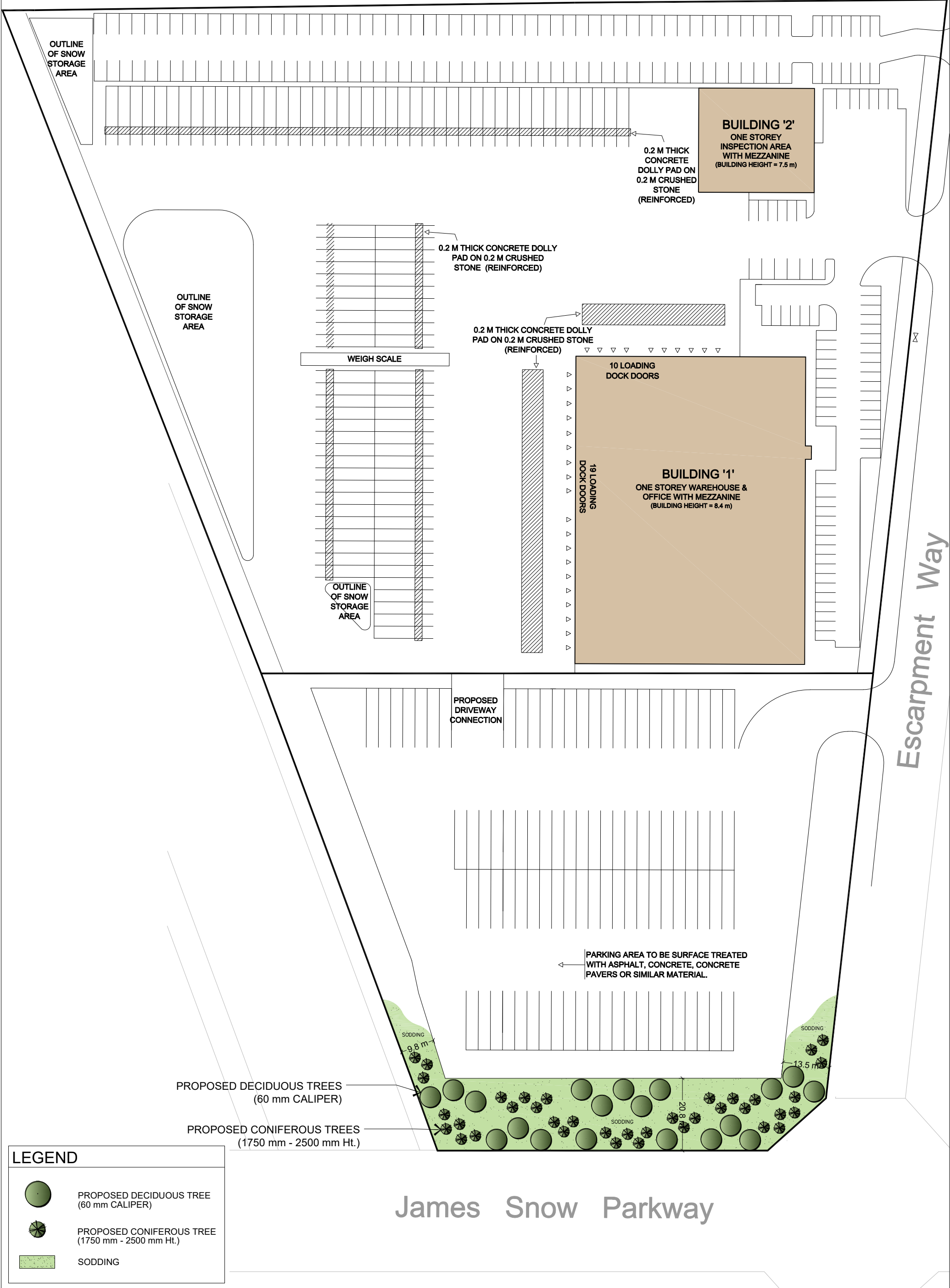
Respectfully submitted,

Rachel Suffern, MPA, M.Sc., MCIP, RPP
Senior Planner, Development Review

For questions, please contact: Rachel.Suffern@Milton.ca Phone: Ext. 2263

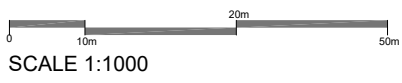
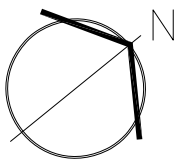
Attachments

Figure 1 – Site Plan and Landscape Plan



CONCEPTUAL LANDSCAPE BUFFER PLAN
8600 ESCARPMENT WAY
MILTON, ONTARIO

PROJECT: 6223
DATE: 13 MAY, 2026





The Corporation of the Town of Milton

Report To: Committee of Adjustment and Consent

From: Rachel Suffern, MPA, M.Sc., MCIP, RPP

Date: June 25, 2026

File No: A26-025M

Subject: 1190 Bronte Street South

Recommendation: THAT the application for minor variance BE APPROVED SUBJECT TO THE FOLLOWING CONDITION:

1. That Site Plan Approval be obtained within two years from the date of decision.

General Description of Application

Under Section 45(1) of the Planning Act, the following minor variances to Zoning By-law 016-2014, as amended, has been requested to:

- Allow a minimum landscape buffer of 0.0 metre abutting a street line (being Louis St. Laurent South and Bronte Street South) (whereas 4.5 metres is required);
- Allow a minimum landscape buffer of 1.0 metres abutting a street long (being Bronte Street South) (whereas 4.5 metres is required)
- Allow a minimum front yard setback of 4.4 metres (whereas 4.5 metres is required);
- Allow a minimum of 7 loading spaces (whereas 8 is required);
- Allow an ornamental projection to encroach 1.0 metre into an exterior side yard; and,
- Allow a parking rate of 1 parking space per 23 square metres (whereas 1 per 20 square metres is required).

The subject property is known municipally as 1190 Bronte Street South and is located at the south-west corner of Louis St. Laurent Avenue and Bronte Street South. Surrounding land uses include a rail corridor to the west, and commercial uses to the north and east.

The proponent has submitted a site plan application (file no. SP-29/22) and Staff is satisfied that it has advanced to a point where it is appropriate to consider a minor variance application. The development proposes nine commercial buildings, including six that are intended to being multi-tenanted and three being single tenant (including an anchor grocer - referred to as Building A on the site plan attached as Figure 1 to this Report).

Official Plan Designation (including any applicable Secondary Plan designations)

The subject property is designated

Official Plan Designation (including any applicable Secondary Plan designations)

The Subject Property is designated as Residential Area with a Secondary Mixed Use Node designation on Schedule A - Urban Land Use Plan and Secondary Mixed Use Node on Schedule C.10.C - Boyne Survey Secondary Plan Land Use Plan within the Town of Milton Official Plan and Boyne Survey Secondary Plan.

The uses being proposed on the Subject Property are all permitted within the applicable designations and the required commercial gross floor area is being provided. As such, it is Staff's opinion that the proposal is in conformity with the Town of Milton Official Plan.

Zoning

The subject lands are zoned as a site-specific Secondary Mixed Use (C2*329) Zone within the Town of Milton Zoning By-law 016-2014, as amended.

The proponent is seeking relief from the following sections:

- Section 7.2 Table 7D: Seeking to allow a minimum landscape buffer of 0.0 metre abutting a street line (being Louis St. Laurent South and Bronte Street South), whereas 4.5 metres is required;
- Section 7.2 Table 7D: Seeking to allow a minimum landscape buffer of 1.0 metres abutting a street long (being Bronte Street South), whereas 4.5 metres is required;
- Section 7.2 Table 7D: Seeking to allow a minimum front yard setback of 4.4 metres, whereas 4.5 metres is required;
- Section 5.11 i) Table 5J: Seeking a minimum of 7 loading spaces, whereas 8 is required;
- Section 4.19.5 i) Table 4H: Seeking to allow an ornamental projection to encroach 1.0 metre into an exterior side yard; and,
- Section 5.8.2 i) Table 5F: Seeking to allow a parking rate of 1 parking space per 23 square metres, where as 1 per 20 square metres is required.

Beyond the relief being requested above, the proposal maintains all of provisions within the Zoning By-law, as amended.

Consultation

Public Consultation

Notice for the hearing was provided pursuant to the Planning Act on, June 9, 2026. As of the writing of this report on, June 18, 2026, Staff have not received any comments from members of the public.

Agency Consultation

No objections were filed with respect to the variance application from Town Staff or external agencies.

Planning Staff acknowledge Parks Planning's comments and are continuing to work with the proponent to provide enhanced landscaping along the right-of-ways. Staff also note that an

Consultation

outdoor amenity area is being provided at the corner of Louis St. Laurent Avenue and Bronte Street South (abutting Buildings E and F) and further south on the Subject Property, a District Park will be provided.

Development Services Comments

The applicant has requested a minor variance to facilitate a commercial plaza that includes nine buildings with vehicular access from Louis St. Laurent Avenue and Bronte Street South. The site plan application is substantially advanced, and all minor variances have been captured through this application.

The relief being requested as it relates to landscape buffers is minor in nature and desirable for the development of the subject lands as the proponent is providing a sidewalk abutting the buildings that have frontage on the specified right-of-ways (in lieu of a complete landscape buffer). The addition of these sidewalks is critical so far as providing clear and safe paths of travel for pedestrians to access and navigate the site. A secondary school is located on the east side of Bronte Street South and a significant number of students are anticipated to be navigating the site during the day. While the landscape buffer is reduced in these areas, Planning Staff is of the opinion that enhanced connectivity improves the overall site and creates a safer pedestrian environment, making the variances desirable for the subject lands. Staff will continue to work with the applicants to provide enhanced landscaping in other areas and are pleased with the outdoor green space that this development will contribute to the Secondary Mixed-Use Node (i.e. amenity area and District Park).

The reduced front yard setback is a result of a pinch point and is minor in nature. 4.0 metres remains an adequate front yard setback and Transportation Staff offer no objection as it relates to the reduced setback, particular so far as obstructing the views of drivers along that right-of-way.

Similarly to the reduced front yard setback, the encroachment of the ornamental features abutting Bronte Street South is minor in nature, does not obstruct pedestrian or vehicular travel (physically or by way views) and contributes to a high-level of urban design for the node.

Finally, the parking reductions are minor in nature and Transportation Planning Staff offer no objections. The reduced number of loading spaces is negligible because the spaces are intended to service multiple buildings within the site, making them shared and providing multiple options to delivery services at any given time. Seven spaces are sufficient in providing the required loading area needed for the buildings and their tenants - a reduction in one space, in this instance, is negligible.

The reduction in the overall parking ratio for the site is negligible. The physical number of spaces provided on the site is 614. The reduction in the parking ratio - from 1 space per 20 square metres to 1 space per 23 square metres - is technical in nature. The by-law requires that spaces that may be obstructed by garden centres and snow storage be removed from the calculation to ensure that appropriate parking be provided year-round; if the spaces, that are otherwise removed from the parking supply calculation due to these seasonal retail areas, were included, the site would maintain the 1 space per 20 square metres rate. The applicant has provided a Transportation Impact Study to review the provided parking and is satisfied that the supply being provided is



Consultation

satisfactory, regardless of if some spaces are temporarily obstructed for seasonal accommodations. Staff also advise that snow storage and garden centre areas have been delineated on the outer limits of the parking lots and ensure spaces closest to the entry ways are available year-round.

Based on the aforementioned, Planning Staff is of the opinion that the variances being requested are minor in nature, desirable for the development of the subject lands, maintains the intent of the Zoning By-law and conforms to the Official Plan.

Respectfully submitted,

Rachel Suffern, MPA, M.Sc.(PI), MCIP, RPP
Planner, Development Review

For questions, please contact: Rachel.Suffern@Milton.ca Phone: Ext. 2263

Attachments

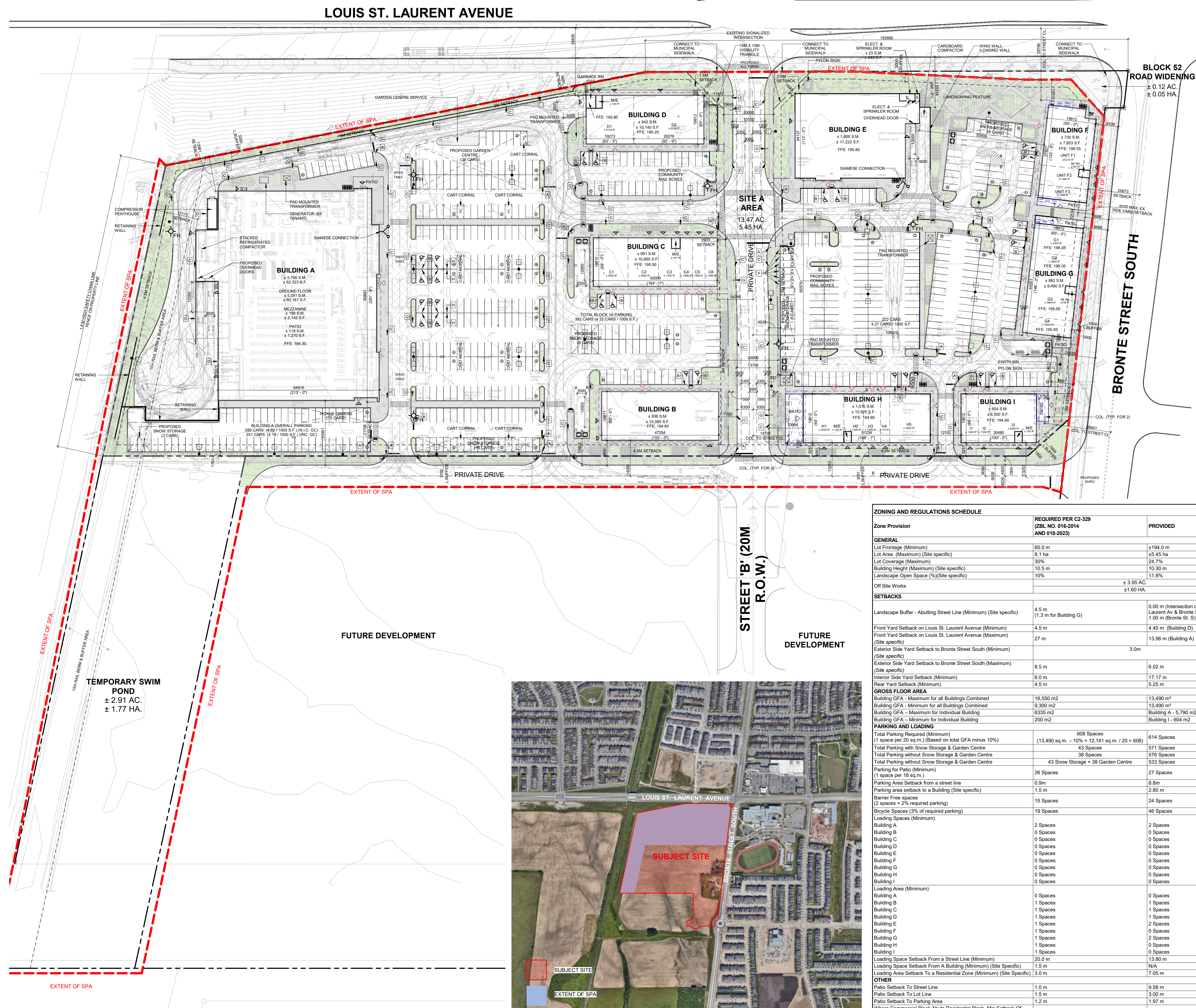
Figure 1 - Site Plan

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LEGEND

- ENTRANCE
- PRIMARY ENTRANCE
- EXIT
- FIRE HYDRANT
- SIGNAGE
- SIAMISE CONNECTION
- FIRE & TRUCK ROUTE (HEAVY DUTY ASPHALT)
- LANDSCAPING
- VISIBILITY TRIANGLE
- LOADING AREA
- LINE OF CORNICE ABOVE

TOWN OF MILTON DEVELOPMENT SERVICES
ZONING REVIEWED FOR C OF A
sherri.jamieson MAY 25, 2026
ZONING OFFICER DATE



SITE BUILDING STATISTICS

Category	± 13.47 AC.	± 5.45 HA.
SITE A AREA	± 3.95 AC.	± 1.60 HA.
OFF SITE WORKS	±62,323 S.F.	±5,790 S.M.
BUILDING A GFA	±2,142 S.F.	±199 S.M.
LEASABLE AREA	±1,270 S.F.	±118 S.M.
MEZZANINE AREA	±63,593 S.F.	±5,908 S.M.
PATIO AREA	±10,080 S.F.	±936 S.M.
BUILDING B GFA	±10,080 S.F.	±936 S.M.
LEASABLE AREA	±10,080 S.F.	±936 S.M.
BUILDING C GFA	±10,080 S.F.	±936 S.M.
LEASABLE AREA	±10,080 S.F.	±936 S.M.
BUILDING D GFA	±10,080 S.F.	±936 S.M.
LEASABLE AREA	±10,080 S.F.	±936 S.M.
BUILDING E GFA	±17,222 S.F.	±1,600 S.M.
LEASABLE AREA	±16,972 S.F.	±1,577 S.M.
BUILDING F GFA	±7,855 S.F.	±730 S.M.
LEASABLE AREA	±7,701 S.F.	±715 S.M.
BUILDING G GFA	±8,493 S.F.	±789 S.M.
LEASABLE AREA	±8,493 S.F.	±789 S.M.
BUILDING H GFA	±10,475 S.F.	±973 S.M.
LEASABLE AREA	±10,475 S.F.	±973 S.M.
BUILDING I GFA	±10,475 S.F.	±973 S.M.
LEASABLE AREA	±10,475 S.F.	±973 S.M.
TOTAL GROSS FLOOR AREA	±145,198 S.F.	±13,490 S.M.
TOTAL COVERAGE	±145,198 S.F.	±13,490 S.M.
TOTAL FSI	±4,965 S.F.	±462 S.M.
TOTAL PATIO AREA	±12,425 S.F.	±1,154 S.M.
TOTAL GROSS FLOOR AREA - 10%	±14,519.8 S.F.	±1,349.0 S.M.
TOTAL PARKING REQUIRED	634 CARS	5.22 CARS/1000 S.F.
1 PER 18 S.M. (PATIO)	26 CARS	
1 PER 20 S.M. (GFA)	608 CARS	
TOTAL PARKING PROVIDED	614 CARS	5.06 CARS/1000 S.F.
SNOW STORAGE	43 CARS	
GARDEN CENTRE	38 CARS	
PATIO PARKING PROVIDED	26 CARS	
(PARKING W/O SNOW STORAGE AND GARDEN AREA)	533 CARS	
TOTAL BICYCLE SPACES REQUIRED	19 SPACES	
TOTAL BICYCLE SPACES PROVIDED	46 SPACES	
BARRIER FREE SPACES REQUIRED	15 SPACES	
2 SPACES + 2% OF REQUIRED PARKING SPACES	24 CARS	
BARRIER FREE SPACES PROVIDED	24 CARS	
EV SPACES	6 CARS	

ZONING AND REGULATIONS SCHEDULE

Zone Provision	REQUIRED PER C2-329 (ZBL NO. 019-2014 AND 019-2023)	PROVIDED
GENERAL		
Lot Frontage (Minimum)	60.0 m	±194.0 m
Lot Area (Maximum) (Site specific)	8.1 ha	±5.45 ha
Lot Coverage (Maximum)	30%	24.7%
Building Height (Maximum) (Site specific)	10.30 m	11.8%
Landscape Open Space (%)(Site specific)		± 3.95 AC.
Off Site Works		±1.60 HA.
SETBACKS		
Landscape Buffer - Abutting Street Line (Minimum) (Site specific)	4.5 m (1.3 m for Building G)	0.00 m (Intersection of Louis St. Laurent Av & Bronte St.) 1.00 m (Bronte St. S)
Front Yard Setback on Louis St. Laurent Avenue (Minimum)	4.5 m	4.45 m (Building D)
Front Yard Setback on Louis St. Laurent Avenue (Maximum)	27 m	13.96 m (Building A)
Exterior Side Yard Setback to Bronte Street South (Minimum) (Site specific)		3.0m
Exterior Side Yard Setback to Bronte Street South (Maximum) (Site specific)	8.5 m	6.02 m
Interior Side Yard Setback (Minimum)	6.0 m	17.17 m
Rear Yard Setback (Minimum)	4.5 m	5.25 m
GROSS FLOOR AREA		
Building GFA - Maximum for all Buildings Combined	16,550 m ²	13,490 m ²
Building GFA - Minimum for all Buildings Combined	9,300 m ²	13,490 m ²
Building GFA - Maximum for Individual Building	6355 m ²	Building A - 5,790 m ²
Building GFA - Minimum for Individual Building	200 m ²	Building I - 604 m ²
PARKING AND LOADING		
Total Parking Required (Minimum) (1 space per 20 sq. m.) (Based on total GFA minus 10%)	608 Spaces	614 Spaces
Total Parking with Snow Storage & Garden Centre	(13,490 sq. m. - 10% = 12,141 sq. m. / 20 = 608)	571 Spaces
Total Parking without Snow Storage & Garden Centre	43 Spaces	576 Spaces
Total Parking without Snow Storage & Garden Centre	38 Spaces	533 Spaces
Total Parking without Snow Storage & Garden Centre	43 Snow Storage + 38 Garden Centre	27 Spaces
Parking for Patio (Minimum) (1 space per 18 sq. m.)	26 Spaces	26 Spaces
Parking Area Setback from a street line	0.9m	8.8m
Parking area setback to a Building (Site specific)	1.5 m	2.80 m
Barrier Free spaces (2 spaces + 2% required parking)	15 Spaces	24 Spaces
Bicycle Spaces (3% of required parking)	19 Spaces	46 Spaces
LOADING SPACES (Minimum)		
Building A	2 Spaces	0 Spaces
Building B	0 Spaces	0 Spaces
Building C	0 Spaces	0 Spaces
Building D	0 Spaces	0 Spaces
Building E	0 Spaces	0 Spaces
Building F	0 Spaces	0 Spaces
Building G	0 Spaces	0 Spaces
Building H	0 Spaces	0 Spaces
Building I	0 Spaces	0 Spaces
Loading Area (Minimum)	0 Spaces	0 Spaces
Building A	0 Spaces	1 Spaces
Building B	1 Spaces	1 Spaces
Building C	1 Spaces	1 Spaces
Building D	1 Spaces	1 Spaces
Building E	1 Spaces	2 Spaces
Building F	1 Spaces	0 Spaces
Building G	1 Spaces	0 Spaces
Building H	1 Spaces	0 Spaces
Building I	0 Spaces	0 Spaces
Loading Space Setback From a Street Line (Minimum)	20.0 m	13.80 m
Loading Space Setback From a Building (Minimum) (Site Specific)	1.5 m	N/A
Loading Area Setback To a Residential Zone (Minimum) (Site Specific)	3.0 m	7.05 m
OTHER		
Patio Setback To Street Line	1.0 m	9.58 m
Patio Setback To Lot Line	1.5 m	3.00 m
Patio Setback To Parking Area	1.2 m	1.97 m
Where Commercial Block Abuts Residential Block, Min Setback Of Parking Spaces To Common Lot Line (Site Specific)	0.0 m	1.70 m



1 SITE PLAN
A100 1:750

2 KEY PLAN
A100 1:750

fieldgate

PROPOSED RETAIL DEVELOPMENT

BRONTE STREET S & LOUIS ST. LAURENT AVE.
MILTON, ON L9T 8B4

#SP-29/22 SITE PLAN

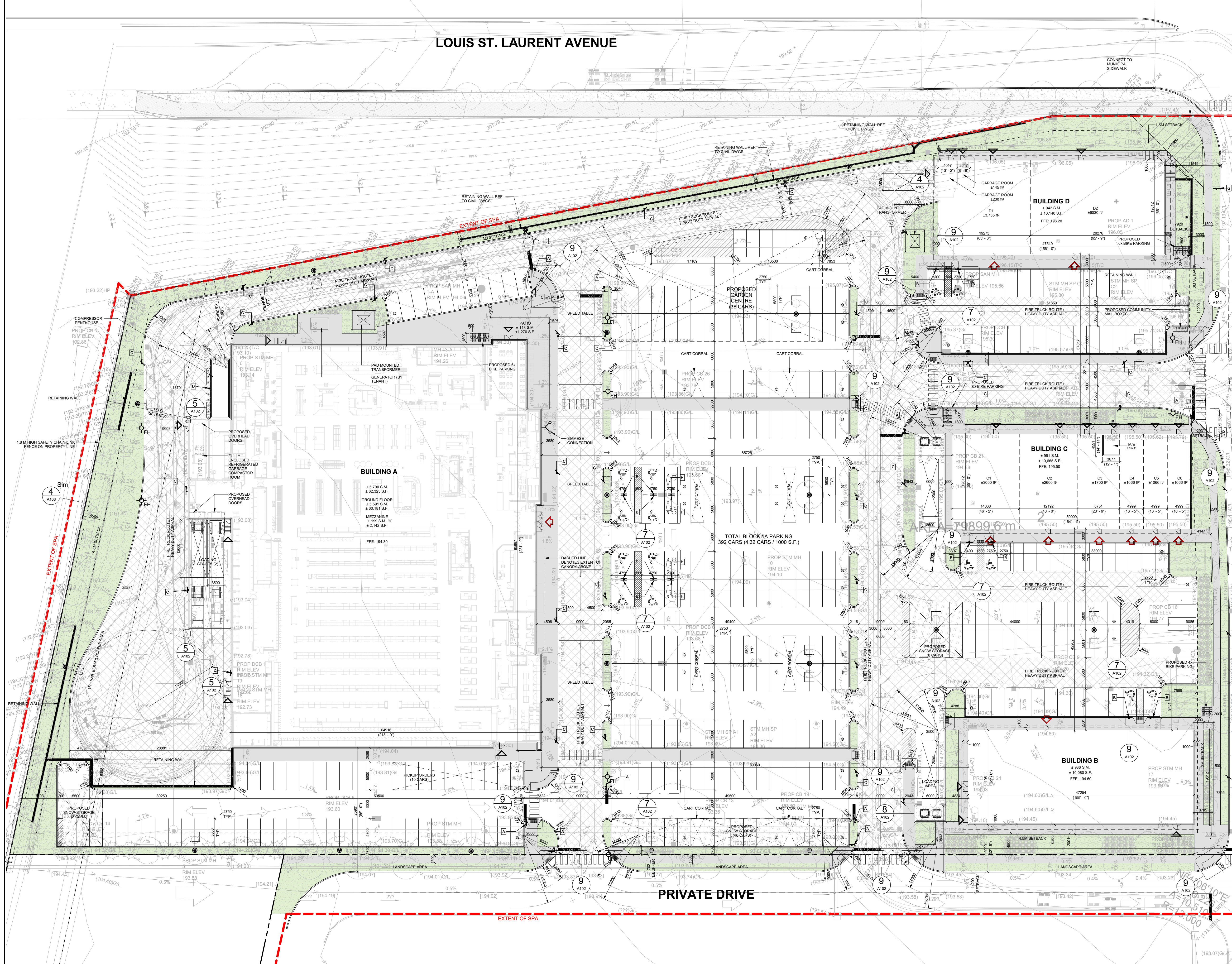
PROJECT NO.	17.042
PROJECT DATE	2024-04-23
DRAWN BY	ETI
CHECKED BY	JJJ
SCALE	1:750
DRAWING NO.	A100
REV	42

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LEGEND

- ENTRANCE
- PRIMARY ENTRANCE
- EXIT
- FIRE HYDRANT
- SIGNAGE
- SIAMISE CONNECTION
- FIRE & TRUCK ROUTE (HEAVY DUTY ASPHALT)
- LANDSCAPING
- VISIBILITY TRIANGLE
- LOADING AREA
- LINE OF CORNICE ABOVE

TOWN OF MILTON DEVELOPMENT SERVICES
ZONING REVIEWED FOR C OF A
sherri.jamieson MAY 25, 2026
ZONING OFFICER DATE



SITE BUILDING STATISTICS

BLOCK 1A	LEASABLE AREA	TOTAL AREA
BUILDING A GFA	±62,323 S.F.	±5,790 S.M.
LEASABLE AREA	±62,323 S.F.	±5,790 S.M.
MEZZANINE AREA	±2,142 S.F.	±199 S.M.
PATIO AREA	±1,270 S.F.	±118 S.M.
TOTAL AREA	±63,593 S.F.	±5,908 S.M.
BUILDING B GFA	±10,080 S.F.	±936 S.M.
LEASABLE AREA	±10,080 S.F.	±936 S.M.
TOTAL AREA	±10,080 S.F.	±936 S.M.
BUILDING C GFA	±10,665 S.F.	±991 S.M.
LEASABLE AREA	±10,498 S.F.	±975 S.M.
ME ROOM	±167 S.F.	±16 S.M.
TOTAL AREA	±10,665 S.F.	±991 S.M.
BUILDING D GFA	±10,140 S.F.	±942 S.M.
LEASABLE AREA	±9,765 S.F.	±907 S.M.
ME ROOM	±145 S.F.	±13 S.M.
GARAGE ROOM	±230 S.F.	±21 S.M.
TOTAL AREA	±10,140 S.F.	±942 S.M.
GROSS FLOOR AREA BLOCK 1A	±93,208 S.F.	±8,659 S.M.
TOTAL PATIO AREA	±1,270 S.F.	±118 S.M.
TGFA WITH PATIO	±94,478 S.F.	±8,777 S.M.

fieldgate

PROPOSED RETAIL DEVELOPMENT
BRONTE STREET S & LOUIS ST. LAURENT AVE.
MILTON, ON L9T 8B4

#SP-29/22 ENLARGED SITE PLAN BLOCK 1A

PROJECT NO.	17.042
PROJECT DATE	2024-04-23
DRAWN BY	ETI
CHECKED BY	JJJ
SCALE	1:375
DRAWING NO.	A101.1
REV	42

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LEGEND

- ENTRANCE
- PRIMARY ENTRANCE
- EXIT
- FIRE HYDRANT
- SIGNAGE
- SIAMESE CONNECTION
- FIRE & TRUCK ROUTE (HEAVY DUTY ASPHALT)
- LANDSCAPING
- VISIBILITY TRIANGLE
- LOADING AREA
- LINE OF CORNICE ABOVE

TOWN OF MILTON DEVELOPMENT SERVICES
MILTON ZONING REVIEWED FOR C of A
sherri.jamieson MAY 25, 2026
ZONING OFFICER DATE



1 SITE PLAN - BLOCK 1B
A101.2 1:375

SITE BUILDING STATISTICS			
BLOCK 1B			
BUILDING E GFA	±17,222 S.F.	±1,000 S.M.	
LEASABLE AREA	±16,962 S.F.	±1,578 S.M.	
M/E ROOM + SPRINKLER ROOM	±240 S.F.	±22 S.M.	
TOTAL AREA	±17,222 S.F.	±1,600 S.M.	
BUILDING F GFA	±7,853 S.F.	±730 S.M.	
LEASABLE AREA	±7,703 S.F.	±716 S.M.	
M/E ROOM	±150 S.F.	±14 S.M.	
PATIO AREA	±640 S.F.	±59 S.M.	
TOTAL AREA	±8,493 S.F.	±789 S.M.	
BUILDING G GFA	±9,490 S.F.	±882 S.M.	
LEASABLE AREA	±9,340 S.F.	±868 S.M.	
M/E ROOM	±150 S.F.	±14 S.M.	
PATIO AREA	±995 S.F.	±92 S.M.	
TOTAL AREA	±10,475 S.F.	±973 S.M.	
BUILDING H GFA	±10,925 S.F.	±1,015 S.M.	
LEASABLE AREA	±10,775 S.F.	±1,001 S.M.	
M/E ROOM	±150 S.F.	±14 S.M.	
PATIO AREA	±1,000 S.F.	±93 S.M.	
TOTAL AREA	±11,925 S.F.	±1,108 S.M.	
BUILDING I GFA	±6,500 S.F.	±604 S.M.	
LEASABLE AREA	±6,350 S.F.	±590 S.M.	
M/E ROOM	±150 S.F.	±14 S.M.	
PATIO AREA	±570 S.F.	±53 S.M.	
TOTAL AREA	±7,070 S.F.	±657 S.M.	
GROSS FLOOR AREA BLOCK 1B	±51,990 S.F.	±4,830 S.M.	
TOTAL PATIO AREA	±3,195 S.F.	±297 S.M.	
TGFA WITH PATIO	±55,185 S.F.	±5,127 S.M.	

fieldgate

PROJECT
PROPOSED RETAIL DEVELOPMENT
BRONTE STREET S & LOUIS ST. LAURENT AVE.
MILTON, ON L9T 8B4

DRAWING
**#SP-29/22 ENLARGED SITE PLAN
BLOCK 1B**

PROJECT NO. 17.042	PROJECT DATE 2024-04-23
DRAWN BY ETI	CHECKED BY JJJ
SCALE 1:375	REV 42



The Corporation of the Town of Milton

Report To: Committee of Adjustment and Consent

From: Olivia Hayes

Date: June 25, 2026

File No: A26-027M

Subject: 11340 Taylor Court

Recommendation: THAT the application for minor variance **BE APPROVED SUBJECT TO THE FOLLOWING CONDITIONS:**

1. THAT the development shall be constructed generally in accordance with the site plan prepared by Ontario Outbuilding and dated and stamped by Town Zoning on June 16, 2026.
2. THAT a Building Permit be obtained within two (2) years from the date of the decision.
3. THAT tree protection fencing shall be installed prior to construction, and maintained for the duration of construction, in accordance with the Arborist Report prepared by Arborwood Tree Service Inc. and dated April 15, 2026.
4. THAT silt fencing shall be installed prior to construction, and maintained for the duration of construction, in accordance with the approved site plan.
5. THAT all mitigation measures recommended in Appendix 7 of the Environmental Impact Assessment prepared by Terrastory Environmental Consulting Inc. and dated June 10, 2026, shall be implemented.
6. THAT the approval be subject to an expiry of two (2) years from the date of decision if the conditions are not met, if the proposed development does not proceed and/or a building permit is not secured.

General Description of Application

Under Section 45(1) of the Planning Act, the following minor variance to Zoning By-law 144-2003, as amended, has been requested to:

- Permit a second attached garage on the lot, whereas the Zoning By-law only permits one garage per lot.

The Subject Property, known municipally as 11340 Taylor Court, is located in the northwest corner of the Hamlet of Brookville. Surrounding uses are rural estate residential, equestrian, and agricultural. The applicant is seeking to construct a second attached garage, projecting from the eastern side of the dwelling, with a storage loft above. As the Zoning By-law only permits one garage per lot in the applicable zone, a minor variance is required. An Environmental Impact

General Description of Application

Assessment (EIA) and Arborist Report have been prepared in support of the application. Planning staff have no concerns with the proposed variance.

Official Plan Designation (including any applicable Secondary Plan designations)

Greenbelt Plan

The Subject Property is designated Greenbelt Protected Countryside Area, and is fully within the Greenbelt Natural Heritage System.

Policy 3.2.2.3 states that new development or site alteration in the Natural Heritage System (as permitted by the policies of this Plan) shall demonstrate that:

- a) There will be no negative impacts on key natural heritage features or key hydrologic features or their functions;
- c) The removal of other natural features not identified as key natural heritage features and key hydrologic features should be avoided. Such features should be incorporated into the planning and design of the proposed use wherever possible;

Halton Region Official Plan

Per Map 1 - Regional Structure, the Subject Property is designated Hamlet, with a Greenbelt Natural Heritage System overlay. Policy 103 establishes that Hamlets are compact rural communities designated to accommodate future residential growth in the rural area and small scale industrial, commercial and institutional uses serving farming and rural communities.

Policy 139.3.6 directs that refinements to the boundaries of the Greenbelt Natural Heritage System are not permitted unless as a result of amendments to the Greenbelt Plan but refinements to the boundaries of the Key Features within the Greenbelt Natural Heritage System may be considered in conjunction with the development of detailed implementation polices in the Local Plans.

Town of Milton Official Plan

The Subject Property is designated Hamlet by Schedule 5 - Rural Structure, and is fully within the Local Natural Heritage System per Schedule 3 - Municipal Structure. The Subject Property also falls within the Brookville Hamlet Secondary Plan, and is designated Hamlet Residential by Schedule C.4.A. Per Schedule 1 - Provincial Land Use Plans, the Subject Property is fully within the Greenbelt Natural Heritage System. Schedule 6 - Green System identifies the entire Subject Property as Key Features of the Local Natural Heritage System.

Official Plan Designation (including any applicable Secondary Plan designations)

Policy 9.3.3.3 directs that within the Hamlet Residential Area designation, permitted uses shall be single detached dwellings and existing semi-detached and duplex dwellings subject to the Zoning by-law of the Town, plus other select uses in conformity with applicable policies. Similarly, within the Local Natural Heritage System, policy 5.2.2.1 permits single detached dwellings on existing lots and accessory buildings.

Policy 5.2.3.1 states that the Town shall apply a systems-based approach to implementing the Local Natural Heritage System by (b) prohibiting development or site alteration within lands adjacent to significant wetlands, significant woodlands, significant valleylands, significant wildlife habitat, significant areas of natural and scientific interest, and fish habitat, unless the ecological function of the adjacent lands has been evaluated and it has been demonstrated that there will be no negative impacts on the natural features or on their ecological functions.

Policy 5.2.3.3(c) allows the Town to require an Environmental Impact Assessment (EIA) for proposed development located wholly or partially inside or within 120 m of the Local Natural Heritage System.

Policy 5.2.3.12(b) establishes that the boundaries of the Local Natural Heritage System may be refined with additions, deletions and/or boundary adjustments through an individual EIA accepted by the Town.

Within the Greenbelt Area, policy 5.3.1.5 echoes that Refinements to the boundaries of the Greenbelt Natural Heritage System are not permitted unless as a result of amendments to the Greenbelt Plan but refinements to the boundaries of the key features within the Greenbelt Natural Heritage System may be considered through a Subwatershed Study, an individual EIA, or similar studies accepted by the Town and Region.

Policy 5.3.1.6(f) states that in accordance with the policies of the Greenbelt Plan and the policies of this Plan, the Town shall permit without the requirement of an EIA the expansion of existing agricultural buildings and structures, residential dwellings, and accessory uses to both, within key features, subject to the following being demonstrated to the satisfaction of the Town and the Region:

- i) there is no alternative and the expansion, alteration or establishment is directed away from the key features to the maximum extent possible;
- ii) the impact of the expansion or alteration on the key feature and its functions is minimized to the maximum extent possible, including the pursuit of best management practices to protect and/or restore key natural heritage features and key hydrologic features and functions; and,

It is Staff's opinion that the proposal is in conformity with the Greenbelt Plan, Halton Region Official Plan, and the Town of Milton Official Plan.

Zoning

The Subject Property is zoned Village Residential (RV) per Zoning By-law 144-2003. Within the RV zone, permitted uses are limited to detached dwellings, additional dwelling units, cottage industries or home occupations, and home daycares.

Variance One: Section 4.1.1.2(i) of the Zoning By-law states that a garage or carport is permitted on a lot provided that an attached or detached garage or carport is not already located on a lot. The application proposes a second attached garage, representing an increase of one garage.

Consultation

Public Consultation

Notice for the applicable hearing was provided pursuant to the Planning Act on June 9, 2026. As of the writing of this report on June 17, 2026, staff have not received any comments on the application from members of the public.

Agency Consultation

No objections were filed with respect to the variance application from Town staff or external agencies. Conservation Halton has confirmed they have no concerns with the proposal, and that a Development Clearance Letter will be required.

Development Services Comments

The application seeks to permit a second attached garage on the Subject Property, where an attached garage already exists. The proposed garage would accommodate parking and storage space, with a loft area above. Planning staff have no concerns with the application, and do not anticipate adverse impacts on adjacent properties.

The intent of the Zoning By-law's restriction on garages is to prevent overdevelopment of residential properties and to maintain desirable neighbourhood streetscapes. Through high-quality design, the application maintains this intent. The proposed garage will include windows, articulation, and a varied roofline along the elevation facing Taylor Court, along with loft space above, allowing the development to read as an addition to the dwelling rather than a garage. Moreover, the proposed garage will extend perpendicular to the existing garage, breaking up the total garage massing and preserving visibility of the dwelling's facade. Existing mature trees along the front and interior lot lines will also help screen the second garage from view. Given the lack of adverse impacts and the high quality of design, the proposed variance is desirable and minor in nature.

Official Plan mapping indicated that the hedgerow along the Subject Property's eastern interior lot line forms a Key Feature of the Natural Heritage System (NHS). As the proposed development



Consultation

encroaches into the limits of this hedgerow, both an Arborist Report and an EIA were requested and prepared. The Arborist Report confirms that two trees would be removed to facilitate the proposed development, while the remaining hedgerow can be maintained. Following a site-specific analysis, the EIA concludes that the hedgerow was erroneously included in NHS mapping, and the Key Features of the NHS are limited to the rear of the Subject Property, more than 40 metres from the proposed development. As such, the EIA demonstrates that these two trees do not qualify as Key Features of the NHS. Policies of the Official Plan do allow for the limits of Key Features to be refined through an EIA. Planning staff also recognize that given the configuration of the existing dwelling, the development is proposed in the most suitable location for a garage addition.

Precautionary mitigation measures identified in the EIA have been integrated into the site plan and into the conditions recommended herein. As the EIA demonstrates that the proposed development will have no negative impacts on Key Features of the NHS or their functions, and that the proposed development will project away from Key Features, planning staff are satisfied that the application maintains the general intent of the Official Plan and Greenbelt Plan. Further, the proposed loft and garage represent an appropriate use for the Hamlet Residential Area designation.

In sum, staff are of the opinion that the application is minor in nature, maintains the intent of the Zoning By-law and Official Plan, and is desirable for the appropriate development or use of the land, building or structure.

Respectfully submitted,

Olivia Hayes, B.E.S.
Planner, Development Review

For questions, please contact: Olivia.Hayes@Milton.ca Phone: Ext. 2454

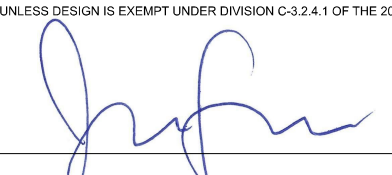
Attachments

Figure 1 – Site Plan

Figure 2 – Architectural Drawings




TOWN OF MILTON
 DEVELOPMENT SERVICES
 RV, GA ZONE
ZONING: REVIEWED FOR C of A
 yaseen.albarim MAY 15, 2026
 ZONING OFFICER DATE

QUALIFICATION INFORMATION
REQUIRED UNLESS DESIGN IS EXEMPT UNDER DIVISION C-3.2.4.1 OF THE 2012 O.B.C.

 JAKOB FABER, BCIN 114291
 ELEVATE HOME DESIGN INC., BCIN 118456
THE UNDERSIGNED HAS REVIEWED AND TAKES RESPONSIBILITY FOR THIS DESIGN AND HAS THE QUALIFICATIONS AND MEETS THE REQUIREMENTS SET OUT IN THE ONTARIO BUILDING CODE TO BE A DESIGNER.




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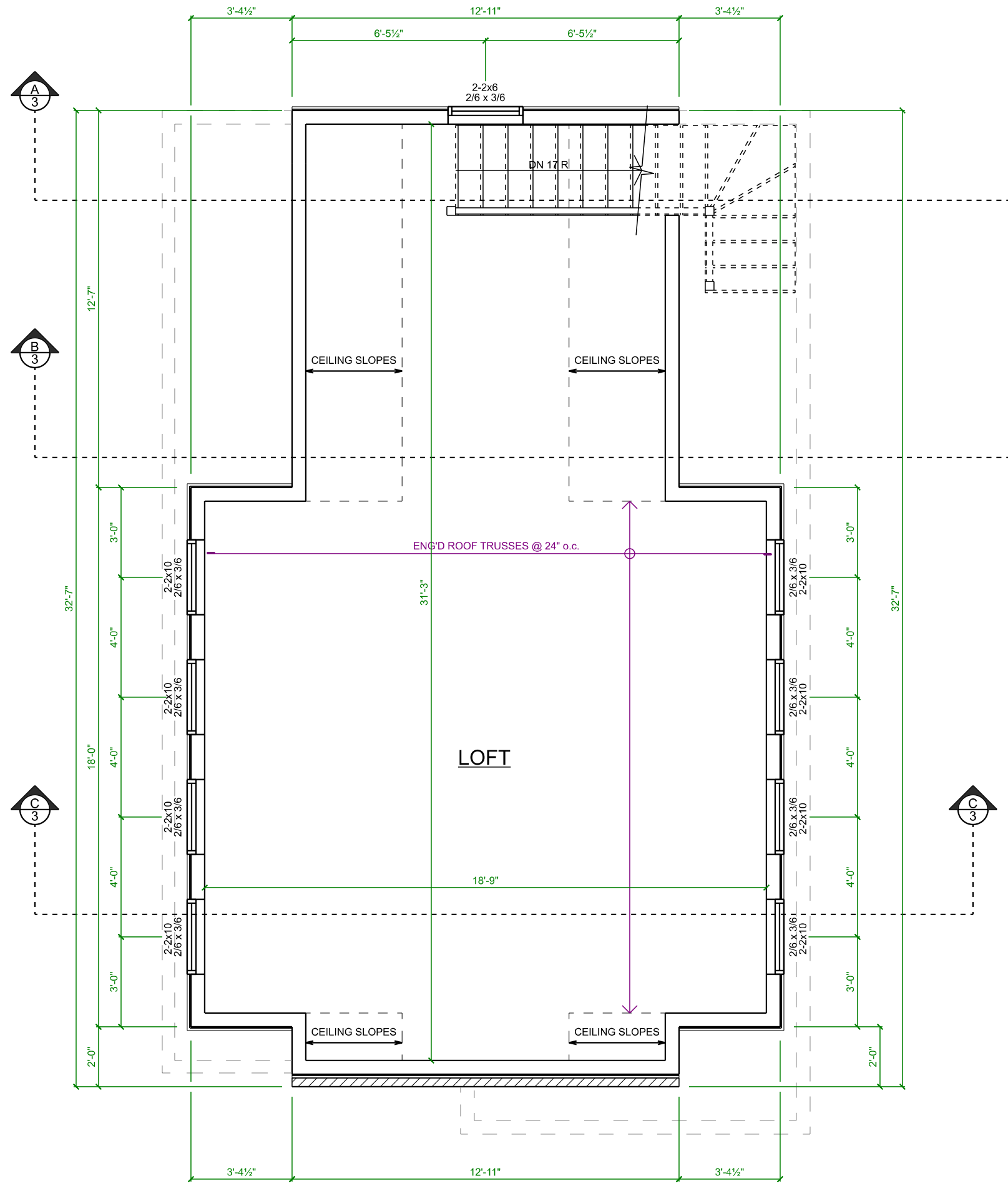
GARAGE ADDITION
 SONNY KENTH
 11340 TAYLOR CT.
 MILTON, ON

CONCEPTS

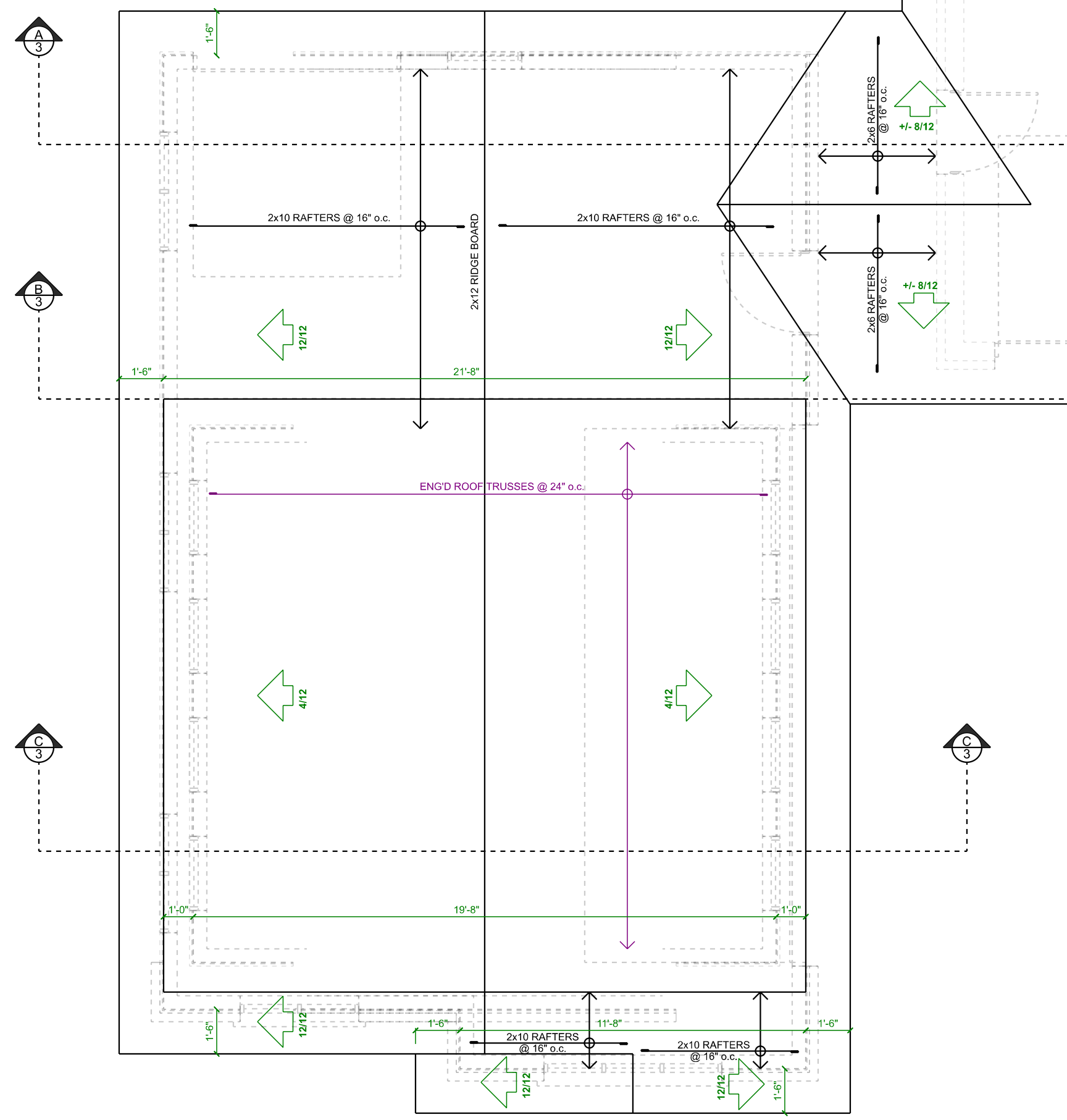
PROJECT NO: 25-109
 STARTING DATE: Dec 15, 2025
 LAST REVISION DATE: Feb 13, 2026
 DRAWN BY: J.F.

SCALE: NOT TO SCALE

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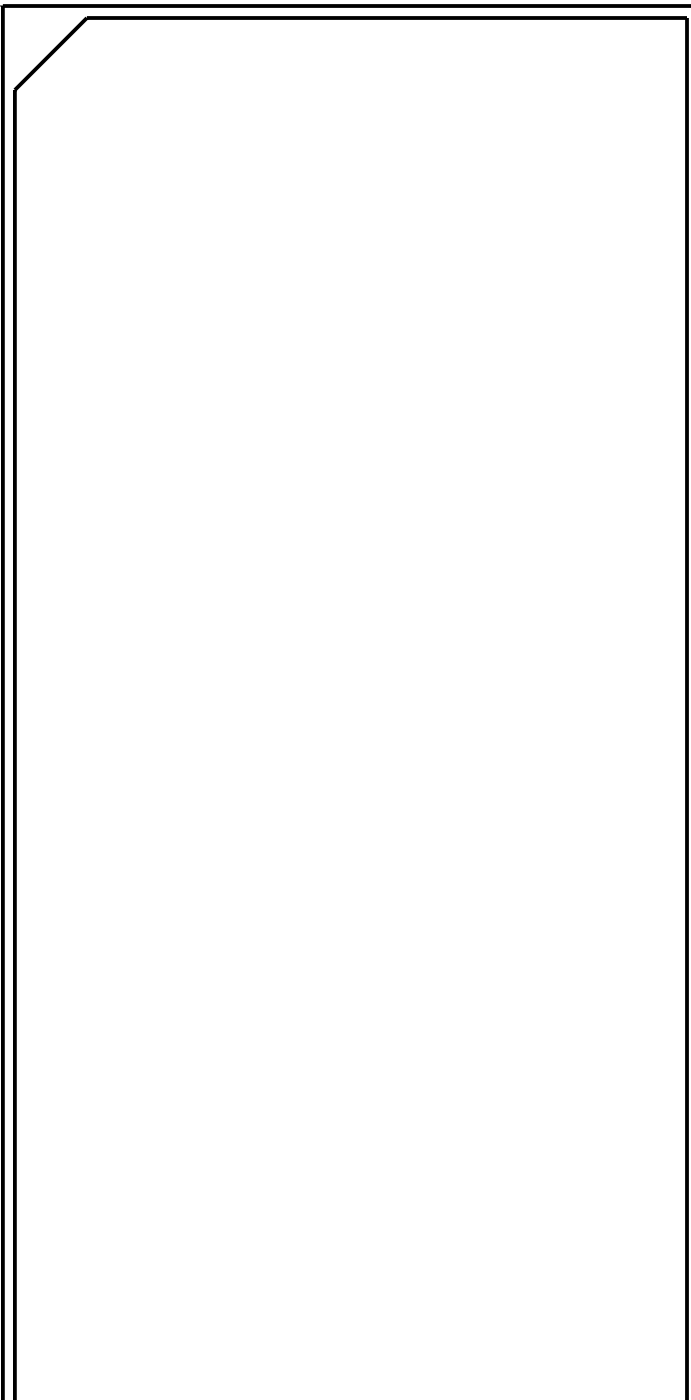


LOFT PLAN
SCALE: 1/4" = 1'-0"



ROOF PLAN
SCALE: 1/4" = 1'-0"

TOWN OF MILTON
DEVELOPMENT SERVICES
RV, GA ZONE
ZONING: REVIEWED FOR C of A
yaseen.albarim MAY 15, 2026
ZONING OFFICER DATE



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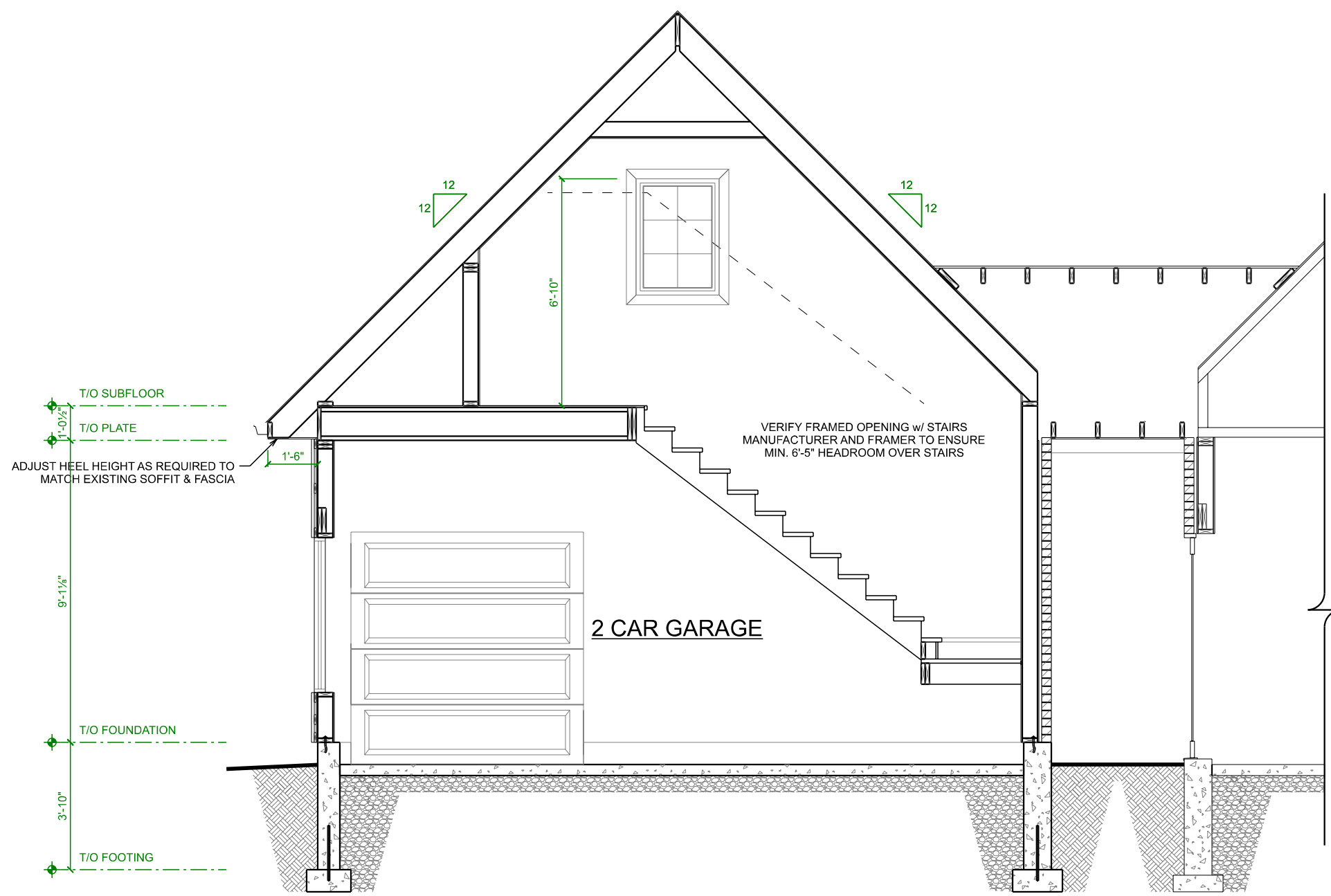
GARAGE ADDITION
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MILTON, ON

LOFT & ROOF PLANS

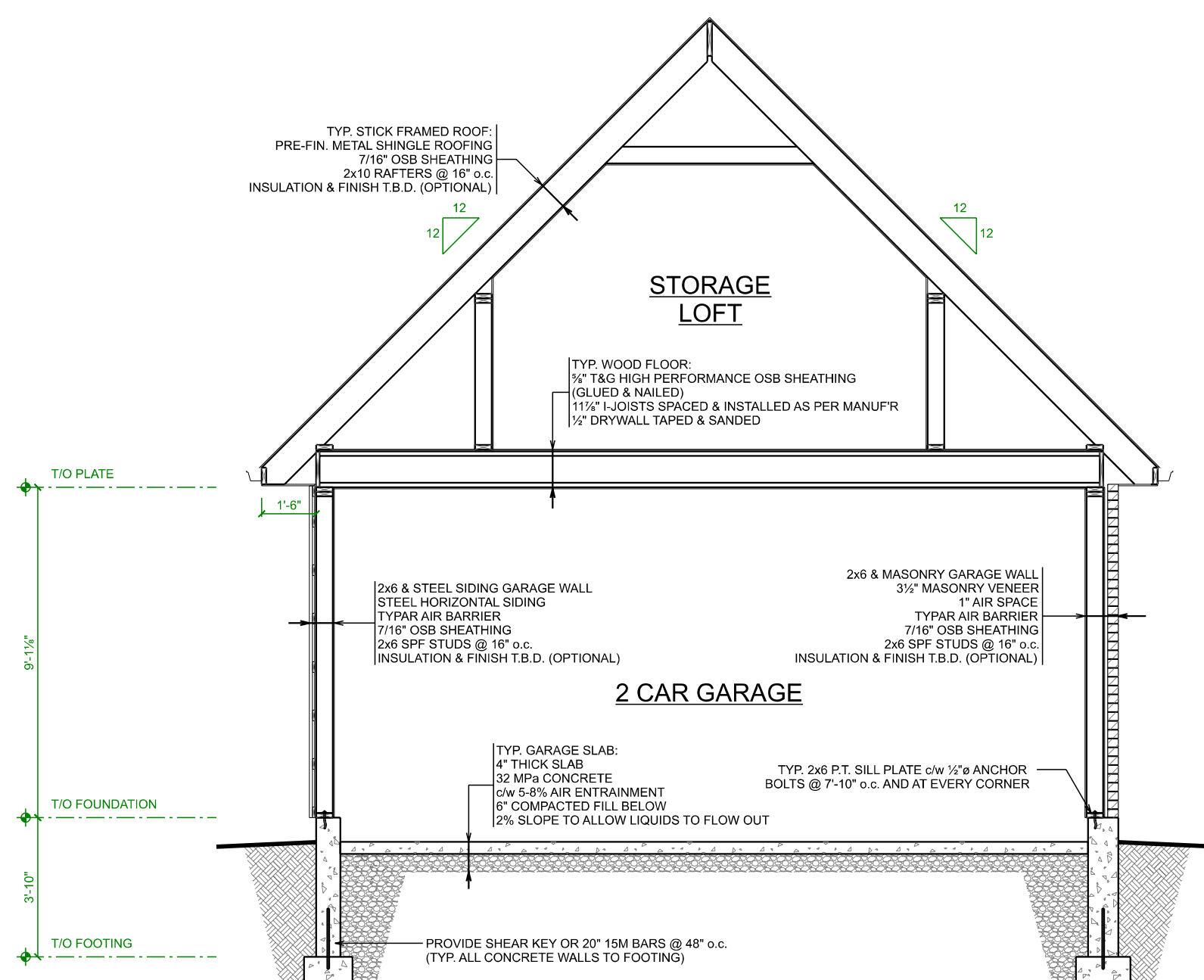
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2
SCALE: 1/4" = 1'-0"

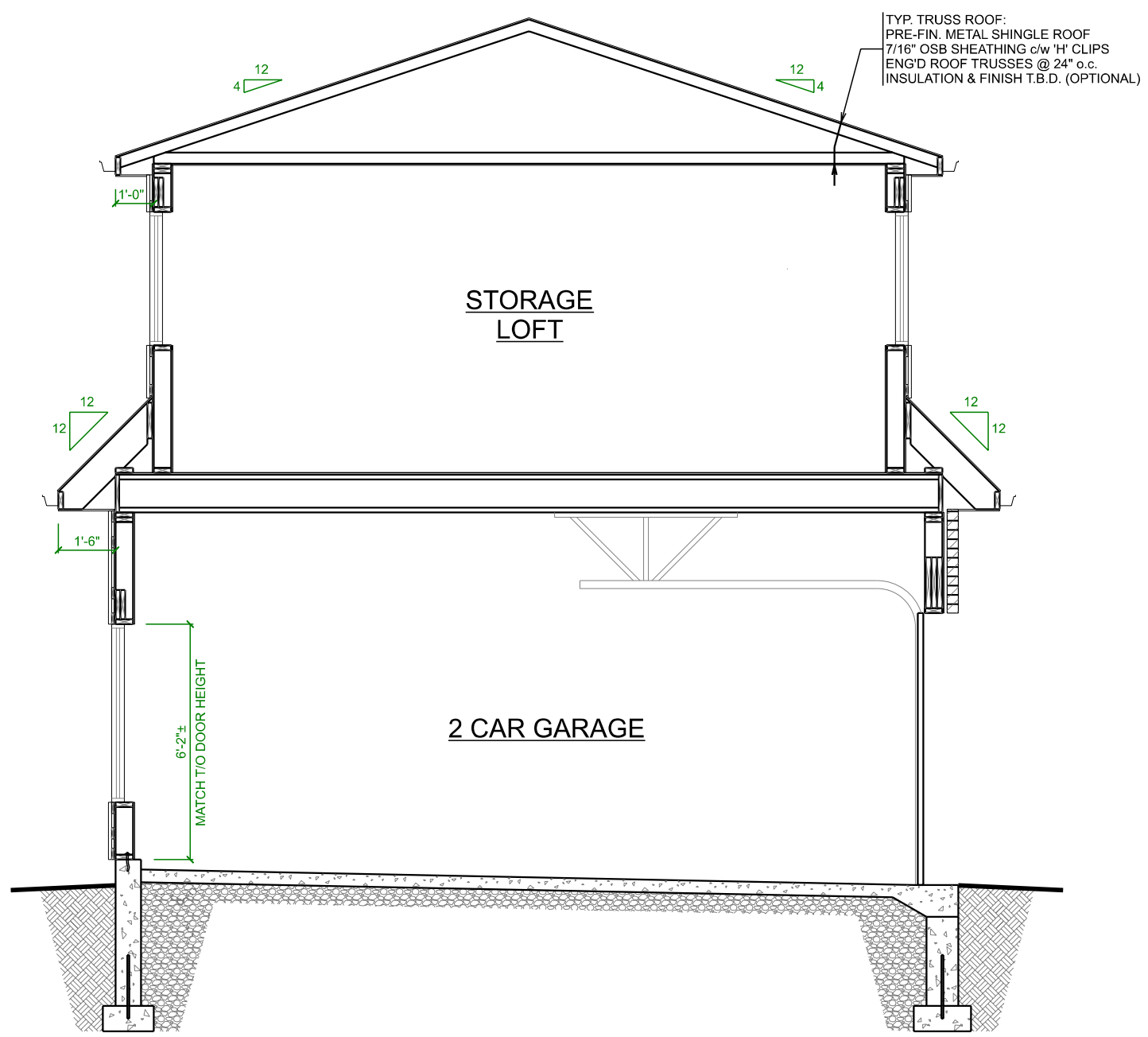
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A
1
CROSS SECTION 'A'
SCALE: 1/4" = 1'-0"



B
1
CROSS SECTION 'B'
SCALE: 1/4" = 1'-0"



C
1
CROSS SECTION 'C'
SCALE: 1/4" = 1'-0"

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ONTARIO OUTBUILDING

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GARAGE ADDITION
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CROSS SECTIONS

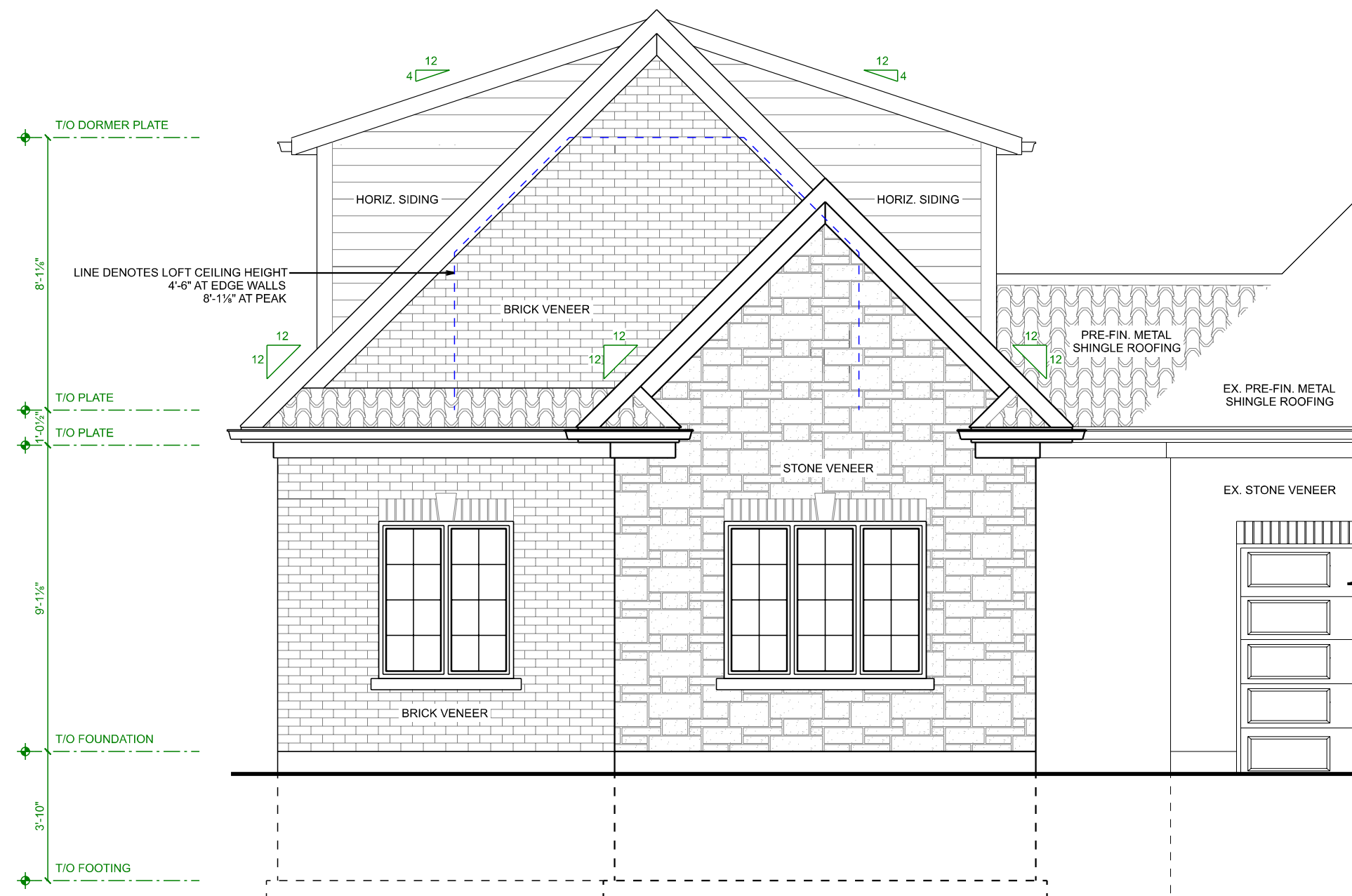
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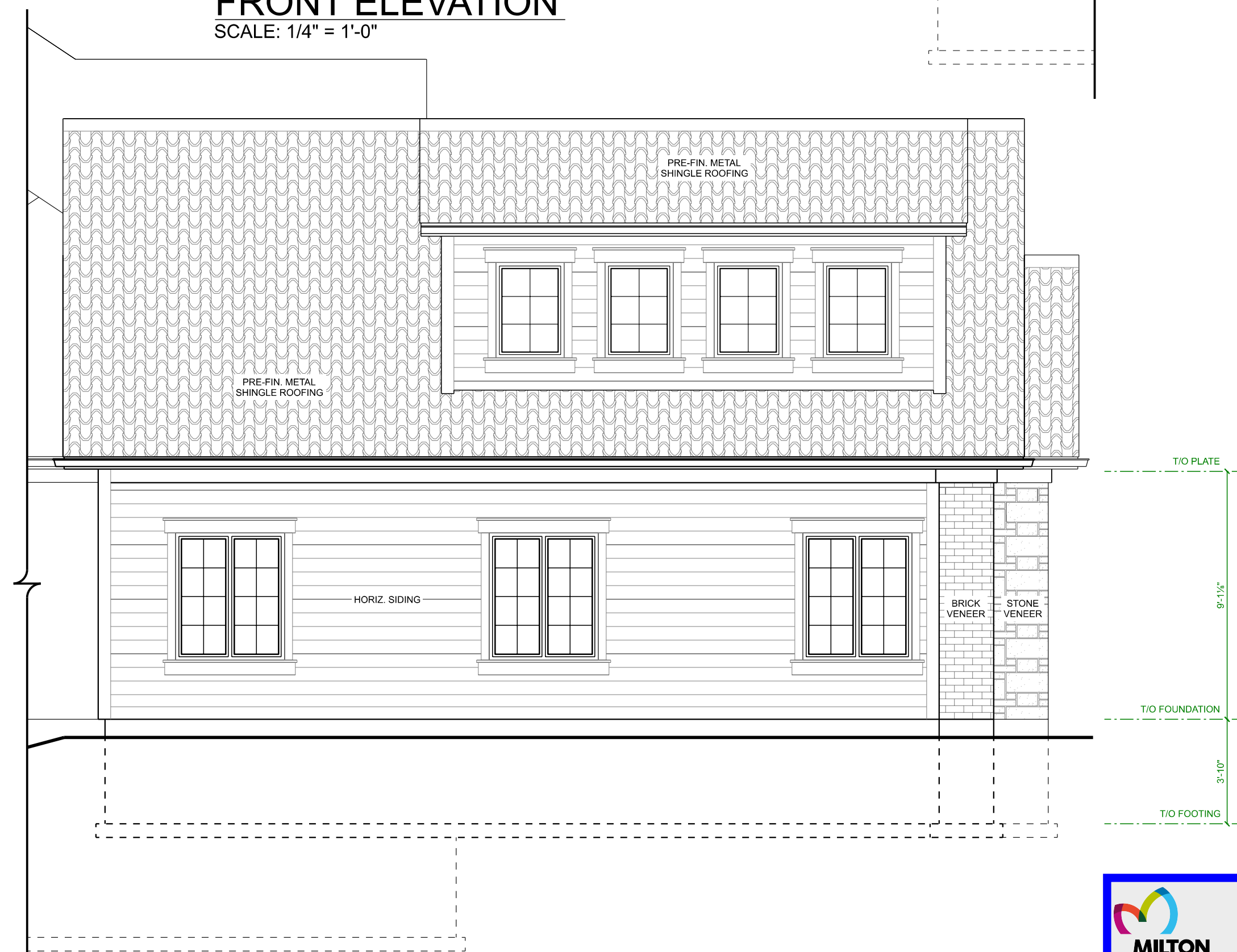
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MILTON TOWN OF MILTON DEVELOPMENT SERVICES
RV, GA ZONE
ZONING: REVIEWED FOR C of A
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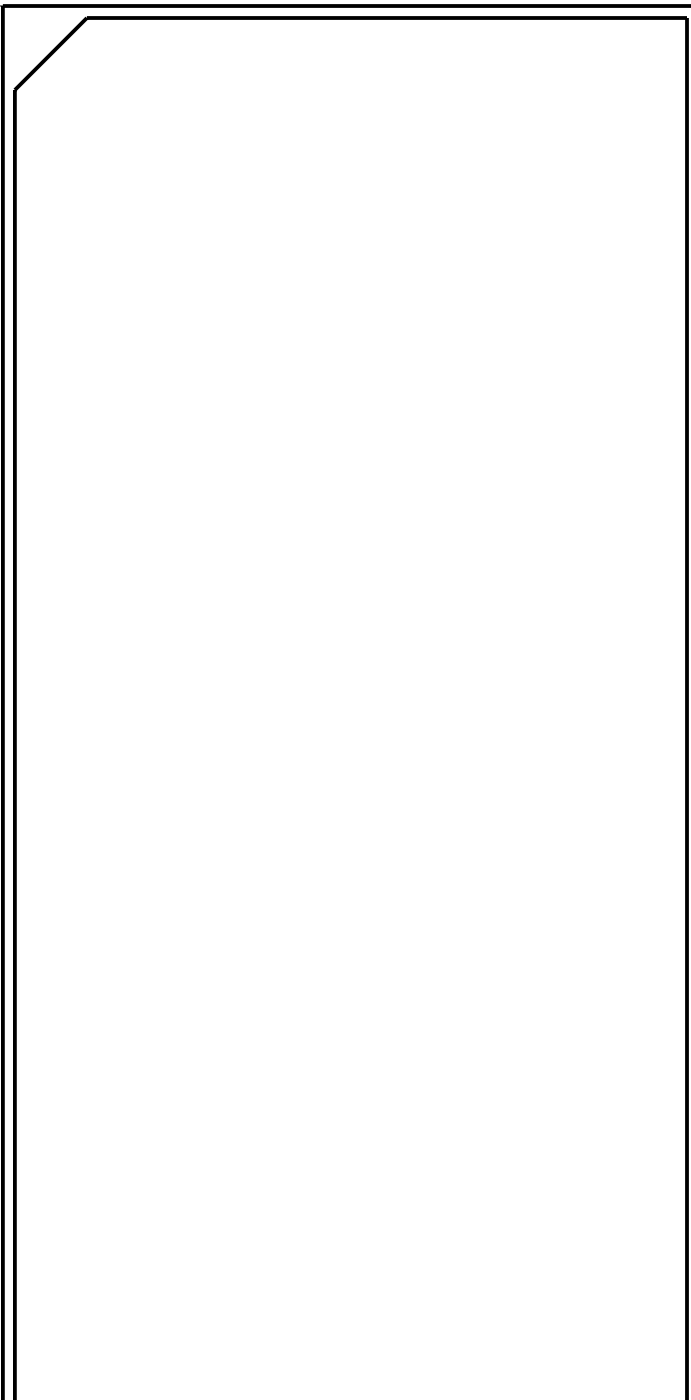


FRONT ELEVATION
SCALE: 1/4" = 1'-0"



LEFT ELEVATION
SCALE: 1/4" = 1'-0"

MILTON TOWN OF MILTON
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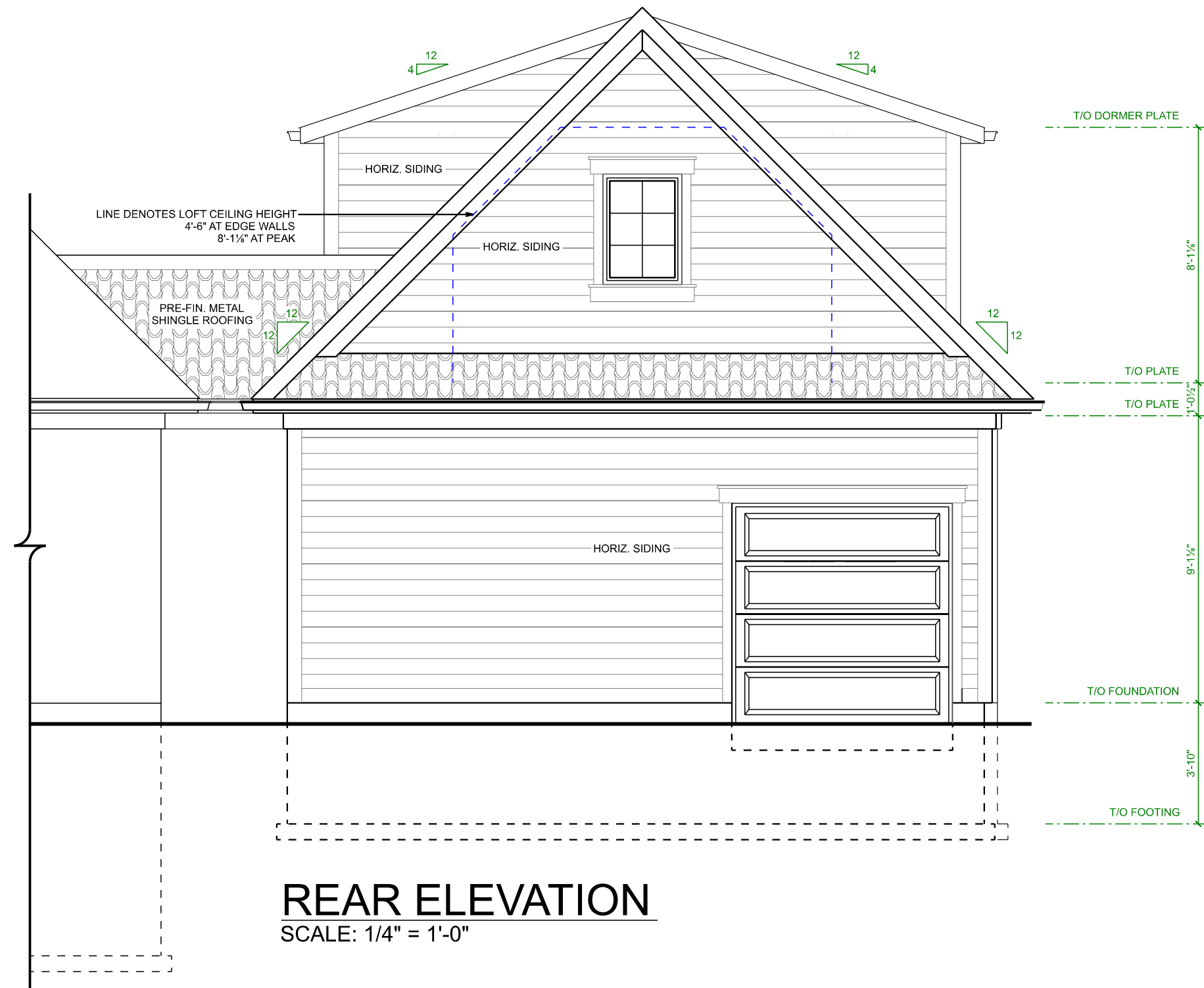
FRONT & LEFT ELEVATIONS

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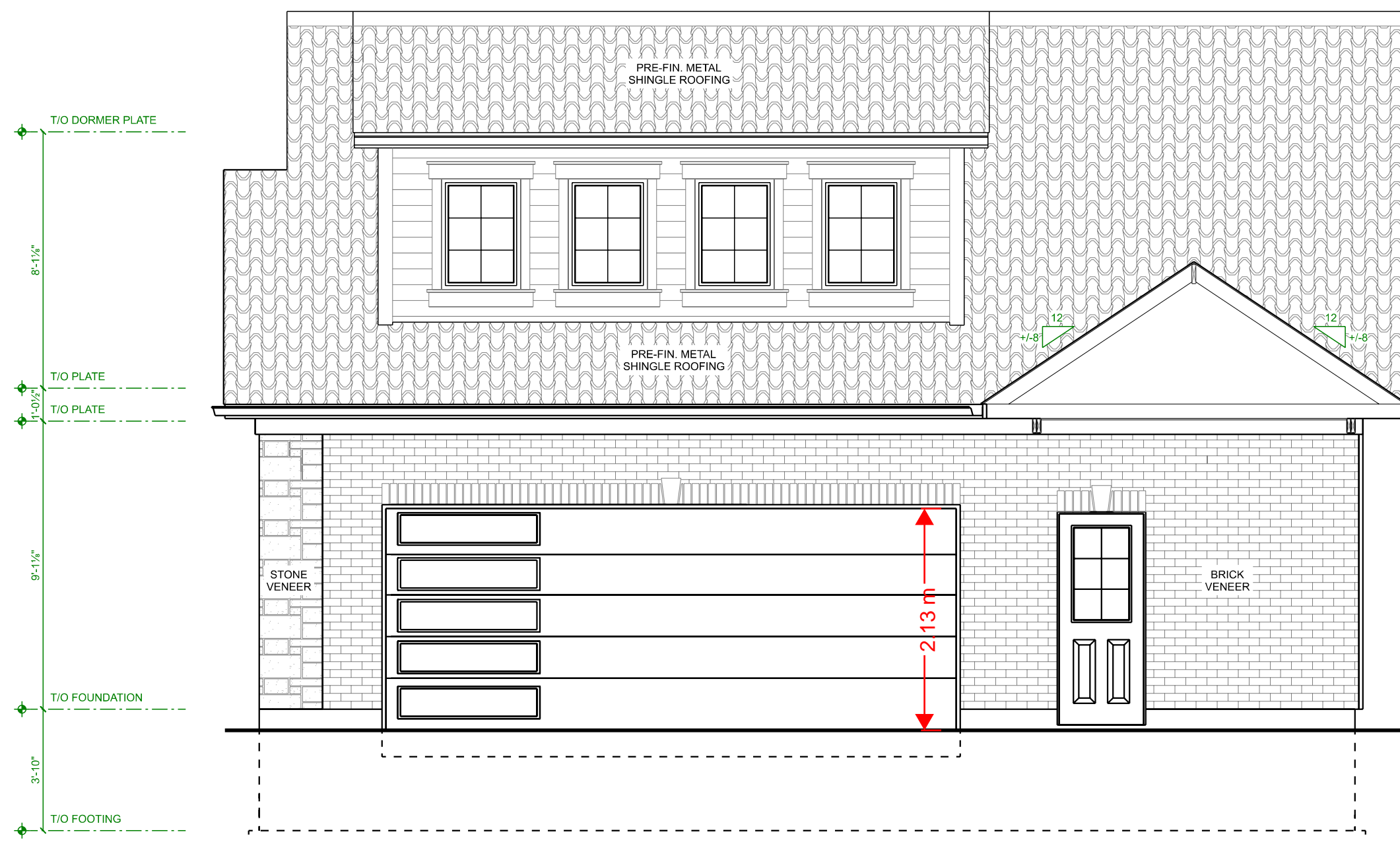
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SCALE: 1/4" = 1'-0"

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REAR ELEVATION
SCALE: 1/4" = 1'-0"



RIGHT ELEVATION
SCALE: 1/4" = 1'-0"

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GARAGE ADDITION

SONNY KENTH
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REAR & RIGHT ELEVATIONS

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OBC 2024 & CONSTRUCTION NOTES

General Construction Notes & Responsibilities

- General Construction Notes
- All dimensions shall be verified on site prior to commencing construction. Report discrepancies or omissions to the designer before proceeding.
- Do not scale drawings. Use written dimensions only. Any missing dimensions shall be confirmed with the designer.
- Contractor shall verify all existing site conditions, utilities, and levels before excavation or layout. Notify designer of any conflicts or site-specific issues.
- Contractor is responsible for compliance with all applicable codes, standards, and municipal requirements, including but not limited to:
 - Ontario Building Code (2024)
 - Local zoning by-laws
 - Utility and site-specific regulations
- Do not assume conditions based on typical details. Where ambiguity or conflict exists, obtain written clarification from the designer before continuing.
- Structural elements shall not be modified without prior written approval by the designer and/or structural engineer.
- Contractor is responsible for ensuring products and assemblies match design intent and are installed per manufacturer specifications.
- All framing members shall be sized, installed, and fastened as per OBC Part 9 or as noted. Any variations must be approved.
- The designer assumes no liability for construction errors, contractor interpretation, or deviations made without written consent.
- Shop drawings, truss layouts, and engineered systems must be submitted to the designer for review before installation.
- The contractor shall maintain a clean, safe job site, and protect adjacent structures and materials during all phases of construction.
- Corrections to drawings due to conflicts, errors, or revisions will be made as necessary, however, the designer's liability is limited to correction of the drawings only, and not for any resulting delays or costs.

Footings, Foundations, Excavation, Radon

- Footings [9.15.3]
 - Soil bearing capacity: 75 kPa (1500 psf) assumed U.O.
 - Minimum footing thickness: 100 mm (4") or equal to projection beyond supported wall/column, whichever is greater
 - Frost protection: Underside of footings to be placed below local frost depth (1.2 m / 47'"); confirm with municipality
 - Consult designer if water table is within a distance below the bearing surface equal to the width of the footing
- Foundation Walls [9.15.4]
 - Extend ≥ 150 mm (6") above finished ground
 - Thickness based on wall height and grade level (refer to 9.15.4.2 tables)
 - Damp proofing required on exterior face of footing to finished grade
- Concrete Specifications [9.3.1]
 - All concrete to meet CSA A23.1 requirements
 - Minimum 28-day compressive strength:
 - Footings/foundation walls: 20 MPa
 - Interior floor slabs (thous): 20 MPa
 - Garage slab/exterior walls: 32 MPa with 5-8% air entrainment
 - Ensure proper curing, especially in cold weather

- Anchorage to Concrete [9.23.6]
 - Anchor bolts (conventional):
 - 12.7 mm (1/2") diameter
 - Embedded 100 mm (3 3/4")
 - Spaced 2.4 m (7'-10 1/4") o.c. and at every corner
 - Anchor bolts (ICF walls - top plates):
 - 12.7 mm (1/2") diameter
 - Embedded 100 mm (3 3/4")
 - Spaced 2.4 m (3'-11") o.c. and at every corner
 - Exterior posts and columns must be securely anchored to resist both uplift and lateral forces

- Drainage [9.14]
 - Weeping tile to be installed around the perimeter of all foundation walls enclosing interior space
 - Diameter: ≥ 100 mm (4")
 - Location: Beside footings, top of pipe below bottom of floor slab
 - Cover: Minimum 150 mm (6") of crushed stone
 - Outlet: Discharge to sump pit, sewer, ditch, or dry well
- Sump Pit
 - Depth: 29" +
 - Area: ≥ 25 m² (2.69 ft²), approx. 22" x 4" diameter
 - List: Air-tight and child-resistant
 - Pump: Automatic pump required where gravity drainage is impractical

- Backfill and Grading [9.12, 9.14]
 - Brace foundation walls before backfilling if lateral support is required or if concrete strength not yet reached
 - Backfill material must be free-draining and free of organic content
 - Site grading must:
 - Prevent water accumulation near the building
 - Not adversely affect adjacent properties
 - Direct water away from well and septic areas
 - Window wells must be drained to the weeping tile

- Radon Protection [9.13.4]
 - Required for all floor and wall assemblies that separate conditioned interior space from the ground
 - Soil gas-resistant air barrier system must comply with OBC 9.25.3 or SB-9
 - Acceptable materials:
 - Polyethylene barrier conforming to CAN/CGSB-51.34-M
 - Floor slab and foundation wall, provided that:
 - All slab-to-wall joints, control joints, and service penetrations are sealed with flexible, durable caulking or gaskets
 - Rough-in for future radon mitigation must include either:
 - A gas-permeable layer + capped vertical vent pipe (min. 100 mm / 4") connected to the interior
 - OR a capped pipe embedded in 100 mm (4") of clean granular fill beneath slab, terminating inside building for future fan

- Insulated Concrete Forms (ICF)
 - All ICF construction to conform to the most recent version of the ICFMA Engineered Prescriptive Design Manual. All references in this section are to this document.
 - Shear walls are req'd and replace the OBC requirement for 4'-0" long wall segments at ea. corner [5.2.4]
 - Refer to ICF Specifications on each floor plan for pertinent requirements

Structural Steel

- Beams & Lintels
 - Minimum steel grade: 350W unless noted otherwise
 - Anchor bolts:
 - Grade 300W (CAN/CSA G40.21) or ASTM A36
 - Top/bottom plates:
 - Grade 300W
 - Welding: Must be performed by CWB-certified welder
 - Provide adequate lateral support to prevent lateral torsional buckling
 - Steel beams must conform to OBC 9.23.4.3
 - Steel columns must conform to OBC 9.17
 - Base/top plates:
 - Minimum 4"x4"
 - Fastened with (2) 3/4" diameter bolts or 2" of 1/2" fillet weld

Wood-Frame Construction [9.23]

- Materials
 - Lumber: Grade-stamped, conforming to CSA O86 and OBC
 - Studs, joists, beams, and lintels: Per OBC tables or CWC Span Book
 - Protect all lumber from contact with concrete or earth
 - Floor joists: Provide bridging or blocking @ 6'-10" o.c.
 - Floor sheathing: OSB or plywood, thickness per 9.23.15.5
- Sill Plates
 - Minimum 2x4 with sill gasket and header wrap
 - Anchor bolts: See Foundation section for specs
- Stud Walls
 - Stud size and spacing: Per OBC Table 9.23.10.1
 - Openings < 3 m (9'-10"):
 - 1 jack stud + 1 king stud (unless noted otherwise)
 - Openings > 3 m (9'-10"):
 - 2 jack studs + 2 king studs (unless noted otherwise)
- Connections
 - All nailed joints per OBC 9.23.3.4
 - Double top plates: Lapped at corners and splices
 - Joist hangers or ledger strips required where joists frame into beams or ledgers
- Engineered Components
 - Trusses, beams, and engineered joists to be installed per engineered drawings
 - Do not cut, notch, or modify engineered components without written approval from the manufacturer
 - Continue in next column

Table 9.23.3.4. - Nailing for Framing

Construction Detail	Min. Nail Length	Min. # or Max. Spacing of Nails
Floor joist or blocking perpendicular to sill plate or top wall plate below - toe nail	3 1/2"	2 per floor joist or blocking
Rim or trimmer joist or blocking (supporting walls with req'd braced wall panels - to sill plate or top wall plate - toe)	3 1/2"	6" o.c.
Wood or metal strapping to underside of floor joists	2 1/2"	2
Cross-bracing to joists	2 1/2"	2 at each end
Double header or trimmer joists	3"	12" o.c.
Floor joist to stud (balloon construction)	3"	2
Ledger strip to wood beam	3 1/2"	2 per joist
Joist to joist splice	3"	2 at each end
Tail joist to adj. header joist (end nailed)	3 1/2"	5
Tail joist to adj. header joist (end nailed) around openings	4"	3
Each header joist to adj. trimmer joist	3 1/2"	2
Each header joist to adj. trimmer joist (end nailed) (around openings)	4"	3
Stud to wall plate (each end) toe nail	2 1/2"	4
Stud to wall plate or end nail	3 1/2"	2
Doubled studs at openings, intersections, or corners	3"	30" o.c.
Doubled top wall plates	3"	24" o.c.
Bottom wall plate to floor framing (exterior)	3 1/2"	18" o.c.
Bottom wall plate (in required braced panels) to floor framing	3 1/2"	6" o.c.
Interior walls to framing or subflooring	3 1/2"	24" o.c.
Braced wall panels in interior walls	3 1/2"	6" o.c.
Horizontal member over openings in non-loadbearing walls	3 1/2"	2
Lintels to studs	3 1/2"	2 at each end
Ceiling joist to plate - toe nail each end	3 1/2"	2
Roof rafter, truss or joist to plate - toe nail	3 1/2"	3
Rafter plate to ceiling joist	4"	2
Rafter to joist (ridge supported)	3"	3
Gusset plate to rafter at peak	2 1/2"	4
Rafter to ridge board - toe nail - end nail	3 1/2"	3
Collar tie to rafter - each end	3"	3
Collar tie lateral support to each collar tie	2 1/2"	2
Jack rafter to hip or valley rafter	3 1/2"	2
Roof strut to rafter	3"	3

COLUMNS

- Columns
 - Must be continuous down to foundation or full bearing on beams
 - Solid blocking required through all joint spaces

Building Envelope [9.25]

- Thermal Insulation [9.25.2]
 - Required in all assemblies separating:
 - Conditioned space from unconditioned space
 - Interior from exterior air
 - Interior from soil
 - Air Barrier System [9.25.3]
 - Required for all walls, ceilings, and floors separating interior from exterior or soil
 - Functions:
 - Prevent winter moisture condensation
 - Minimize soil gas entry (e.g., radon)
 - Acceptable materials:
 - Sheet/paper-type air barriers must leak ≤ 0.02 U/(s·m) @ 75 Pa
 - Polyethylene must comply with CAN/CGSB-51.34-M
 - Must be continuous and sealed at:
 - All joints (taped or caulked)
 - Penetrations (sealed with foam, tape, or caulking)
 - Door/window frames (flashing tape or spray foam)
 - Access hatches and sump pit lids (sealed or weather-stripped)
 - If using foundation wall or slab as air barrier:
 - All joints and penetrations must be caulked
 - Vapour Barrier [9.25.4]
 - Required in all thermally insulated assemblies
 - Poly vapour barrier must meet CAN/CGSB-51.34-M
 - Placement:
 - On the warm side of insulation
 - In contact with or immediately adjacent to insulation to prevent condensation

Roofing Materials [9.26]

- Eave Protection
 - Required from roof edge up 900 mm (35 1/4") or to ≥ 300 mm (11 3/4") inside warm wall
 - Not required for:
 - Unheated garages
 - Porches
 - Roofs with slope $\geq 8/12$
- Underlayment
 - No. 15 asphalt felt or equivalent
 - Installed with minimum 50 mm (2") head and end laps
- Asphalt Shingles
 - Minimum roof slope: 4/12 (1 in 3)
 - Starter strips: Factory-made or reversed shingles at eaves
 - Fastening:
 - As per manufacturer
 - Must comply with OBC 9.26.7.2-9.26.7.4

Soffits [9.19.1]

- Ventilation
 - Soffits must supply 50% of required attic ventilation
 - Remaining ventilation to be located near ridge or high point
 - Baffles required to prevent insulation blocking airflow at eaves
 - Where an exhaust outlet is located within a soffit discharging air containing moisture, soffit shall be unvented 71" on each side of the exhaust outlet
- Attic Spaces
 - Ventilation [9.19.1]
 - Required vent area: Minimum 1/300 of insulated area for roof areas with attic
 - Required vent area: Minimum 1/150 of insulated area for roof areas without attic
 - Vent types and locations:
 - Soffit vents
 - Ridge or gable-end vents
 - Maintain ventilation clearance at eaves:
 - ≥ 63 mm (2 1/2") between insulation and roof sheathing
 - When baffles are used, provide ≥ 25 mm (1") unobstructed air space
 - Access Hatches [9.19.2]
 - Required if attic or roof space meets all 3 requirements:
 - ≥ 10 m² (107.6 ft²)
 - ≥ 1000 mm (39 3/4") in length or width
 - ≥ 600 mm (23 1/4") in height
 - Minimum opening size:
 - ≥ 50 x 900 mm (21 1/4" x 35 3/4")
 - May be reduced to 500 mm (19 1/2") in one direction for single dwellings only

Crawl Spaces [9.18]

- Access & Clearance
 - Access opening required
 - Minimum size: 500 mm x 700 mm (19 3/4" x 27 1/2")
- Ground Cover [9.18.3]
 - Install continuous 6 mil polyethylene vapour barrier directly over ground
 - Overlap joints by ≥ 100 mm (4")
 - Seal overlaps with tape or adhesive
- Ventilation [9.18.4]
 - Required for unheated crawl spaces
 - Provide exterior wall vents
 - Minimum vent area:
 - ≥ 1 m² per 50 m² of crawl space floor area
 - Equivalent to 1 ft² per 538 ft²
 - Vents must be located to provide cross-ventilation
- Insulation & Air/Vapour Barrier [9.25.2.2]
 - If crawl space is heated or semi-heated:
 - Insulate foundation walls per SB-12
 - Install air and vapour barriers per OBC 9.25

Exterior Cladding and Finishes [9.27.2]

- Weather Protection
 - Cladding must shed water and be part of a two-stage protection system:
 - First plane: Exterior cladding (e.g., siding, stucco, brick)
 - Second plane: Sheathing membrane or equivalent drainage plane
 - Sheathing membrane:
 - Breather-type, CAN/CGSB-51.32-M
 - Lapped 100 mm (4"), shingled and continuous around openings
- Clearances
 - Moisture-sensitive cladding (e.g., wood, OSB, plywood):
 - ≥ 200 mm (8") above finished grade
 - Cladding above roof:
 - ≥ 50 mm (2") above roof surface
 - Fastening
 - All cladding must be fastened to framing, furring, or blocking (not just insulation unless approved)
 - Vinyl siding:
 - Max 400 mm (15 3/4") o.c. horizontally
 - Nail loosely at slot center to allow for expansion
 - Wood siding:
 - Fasten to each stud or per Table 9.27.5.4-A
 - Use nails ≥ 51 mm (2")
 - Fastener penetration:
 - Vinyl: Minimum 1 1/4" into framing
 - All other cladding: Minimum 1" into framing
 - All fasteners must be corrosion-resistant for exterior use
 - Furring (Rainscreen)
 - Not mandatory unless required by cladding system
 - If used:
 - Minimum 19x38 mm (1x2) lumber
- Flashing
 - Required at:
 - Every horizontal junction of cladding
 - Roof/wall intersections
 - Above and below windows and doors
 - Transitions between different substrate materials
 - Flashing requirements:
 - Extend ≥ 50 mm behind sheathing membrane
 - Slope 26% to shed water
 - Include end dams and drip edges
 - Acceptable materials:
 - Aluminum ≥ 0.48 mm
 - Steel ≥ 0.33 mm
 - Vinyl ≥ 1.02 mm
- Masonry Veneer
 - Brick & Stone Veneer [9.20]
 - Minimum 25 mm (1") air space behind veneer
 - Weep holes:
 - Max 800 mm (32") o.c. at base and above lintels
 - Brick ties:
 - Corrosion-resistant
 - Max 800 mm (32") horizontal, 400 mm (16") vertical spacing
 - Support veneer on masonry, concrete, or steel
 - Over openings:
 - Use steel or reinforced concrete lintels or masonry arches
 - Steel lintels:
 - Protected from corrosion (painted or galvanized)
 - Must conform to Table 9.20.2.8 or 9.20.2.4
 - Sealants (Caulking) [9.27.4.1]
 - Seal all required joints to prevent water ingress:
 - Firm joints
 - Penetrations (pipes, vents, fixtures)
 - Joints between dissimilar materials
 - Sealants must conform to one of the following:
 - CAN/CGSB 16.13
 - ASTM C834, C920, C1184, or C1311
 - Use high-quality, UV-resistant, exterior-grade sealant

Table 9.20.5.2-B&C - Max. Span for Steel Lintels Supporting Masonry Veneer

Min. Section Size (inches/mm)	70 mm Brick	90 mm Brick	100 mm Stone
3 1/4" x 3 1/4" (89 x 76 x 3)	8'-4 1/2"	-	-
3 1/2" x 3 1/4" (89 x 89 x 6.3)	8'-5 1/2"	8'-1 1/2"	7'-6 1/2"
4 x 3 1/4" (102 x 89 x 6.3)	9'-1 1/2"	8'-8 1/2"	8'-1 1/2"
5 x 3 1/2" x 5 1/2" (127 x 89 x 7.3)	11'-2 1/2"	10'-10 1/2"	10'-1 1/2"
6 x 3 1/2" x 7 1/2" (127 x 89 x 11.3)	11'-11 1/2"	11'-5"	10'-7 1/2"
5 x 3 1/2" x 12" (127 x 89 x 12.7)	12'-8 1/2"	11'-8 1/2"	10'-11 1/2"
6 x 3 1/2" x 7 1/2" (152 x 89 x 11.3)	13'-3 1/2"	12'-8 1/2"	11'-7 1/2"
6 x 3 1/2" x 12" (152 x 89 x 12.7)	14'-2 1/2"	13'-4 1/2"	12'-4 1/2"
8 x 4 1/2" (152 x 102 x 12.7)	14'-4"	13'-8 1/2"	12'-6 1/2"
7 1/4" x 7 1/2" (178 x 102 x 11.3)	14'-11 1/2"	14'-1 1/2"	13'-1 1/2"
7 x 4 1/2" (178 x 102 x 12.7)	15'-1 1/2"	15'-3 1/2"	13'-11 1/2"
W8x15 (W150 x 22)	13'-10 1/2"	13'-5"	12'-10 1/2"
W8x20 (W150 x 30)	15'-4 1/2"	14'-9 1/2"	14'-2 1/2"
W8x18 (W200 x 27)	17'-3 1/2"	16'-8"	15'-10 1/2"
W8x21 (W200 x 31)	18'-3 1/2"	17'-7 1/2"	16'-9 1/2"
W8x24 (W200 x 36)	18'-8 1/2"	18'-1 1/2"	17'-1 1/2"

Windows and Doors [9.6, 9.7, 9.9.10, 9.14.6]

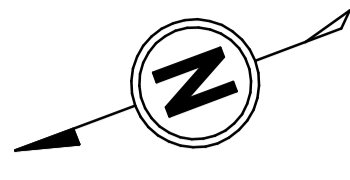
- Egress Windows [9.9.10.1]
 - Required if no exterior door is provided on the same level
 - Minimum operable area: 0.35 m² (3.77 ft²)
 - No dimension less than 380 mm (15")
 - Maximum sill height: 1000 mm (39") above floor (except basements)
 - Must be operable without tools and remain open without hardware support
 - If located in a window well:
 - Minimum clearance in front of open sash: 550 mm (21 5/8")
 - Well covers must be operable from inside without tools
- Window Wells [9.14.6.3]
 - Must be drained to the foundation drain or other approved location
- Safety Glazing [9.6.1.4]
 - Required if:
 - Sliding, storm, and French doors
 - Entry doors with glass ≥ 0.5 m² and bottom edge ≤ 900 mm (35 3/4") from floor
 - Bathroom or shower enclosures (Class A glazing)
 - Sidelights mistaken as doors (e.g., > 500 mm wide and low to the floor)
- Glazing Near Stairs [9.8.8.1.(6)-(7)]
 - Must be drained to the foundation drain or other approved location
 - Class within 900 mm (35 3/8") of stair tread or landing:
 - Must be guarded, or
 - Be non-operable and resist lateral load per Part 4
 - Door Stages [9.5.2.1, 9.5.11]
 - Main entry door:
 - Minimum 810 mm (31 7/8") wide
 - Minimum 1980 mm (78") high
 - Interior doors:
 - Bedrooms and common areas: Minimum 760 mm (30")
 - Bathrooms: Minimum 810 mm (32")
 - Window door requirements
 - Resistance to Forced Entry [9.7.5.3]
 - Windows within 2 m of grade must meet NAFS performance for forced entry
 - Installation and Flashing [9.7.6]
 - Windows and doors must be:
 - Installed level and plumb
 - Properly shimmed and fastened
 - Openings must be flashed and sealed into the building envelope
 - Exterior joints must be caulked

- Masonry Fireplaces [9.22]
 - Masonry Fireplace Construction
 - Firebrick liner required unless listed steel liner used
 - Min. thickness: sides/back 50 mm (2"), floor 25 mm (1")
 - Fireplace wall (incl. firebrick):
 - ≥ 190 mm (7 1/2") if firebrick < 51 mm or steel liner used
 - With steel liner + air space: solid wall ≥ 50 mm (3/4"), hollow wall ≥ 190 mm (7 1/2")
 - Fire chamber depth: ≥ 300 mm (11 3/8")
 - Hearth extension: ≥ 400 mm (15 3/4") front, ≥ 200 mm (7 7/8") sides
 - Combustion air: required for solid-fuel units
 - Non-combustible duct, min. 100 mm (4") \varnothing with damper
 - Smoke chamber wall:
 - ≥ 190 mm (7 1/2"), or ≥ 140 mm (5 1/2") if back exposed to exterior
 - Clearance to combustibles:
 - ≥ 100 mm (4") sides/back, ≥ 50 mm (2") at exterior walls
- Masonry Chimneys [9.21]
 - Wall Thickness
 - Minimum 70 mm (2 3/4") solid masonry units
 - Height Above Roof
 - Must extend:
 - ≥ 500 mm (35 3/8") above the point of roof penetration
 - ≥ 500 mm (35 3/8") above anything within 3.0 m (9'-10") horizontally
 - Lateral Support
 - Not required if:
 - No horizontal dimension < 400 mm (15 3/4")
 - Chimney height above roof or wall ≤ 6 m (11'-9 1/4")
 - If either condition is not met, provide lateral support per Part 4
 - Clearances to Combustibles
 - Inside the building: ≥ 50 mm (2")
 - On exterior walls: ≥ 12 mm (1/2")
 - Chimney Cap
 - Top must be capped with concrete, masonry, or metal
 - Must slope to shed water with a 25 mm (1") drip edge
 - Separation Between Flues
 - Clay liners: ≥ 70 mm (2 3/4")
 - Firebrick liners: ≥ 90 mm (3 1/2")

Stairs, Ramps, Landings, Handrails & Guards [9.8]

- Stair Width and Height
 - Minimum stair width: 860 mm (33 7/8") for single dwelling units or secondary suites
 - Maximum vertical rise per flight: 3.7 m (12'-1 5/8")
- Step Dimensions (Private Stairs)
 - Max. riser height: 200 mm (7 7/8")
 - Min. riser height: 125 mm (4 15/16")
 - Min. tread run: 225 mm (10")
 - Max. tread run: 355 mm (14")
 - Uniformly required:
 - Max 5 mm variation between adjacent risers/treads
 - Max 10 mm variation across an entire flight
 - Continue in next column

- Headroom Over Stairs & Landings
 - Minimum clear height: 1950 mm (6'-4 7/8")
- Landings
 - Minimum size:
 - Equal to stair width
 - Or 1100 mm (43 3/8") if stair turns ≤ 90
- Handrails
 - Are required if:
 - Interior stair has more than 2 risers
 - Exterior stair has more than 3 risers
 - Height: 865-1070 mm (34'-4 1/8" above stair nosings)
 - Must be continuous and graspable
 - Where stair width > 1100 mm (43 3/8"):
 - Handrails required on both sides
 - Continue in next column: Guards (Guardrails)
 - Required if height above adjacent grade or floor > 600 mm (23 5/8")

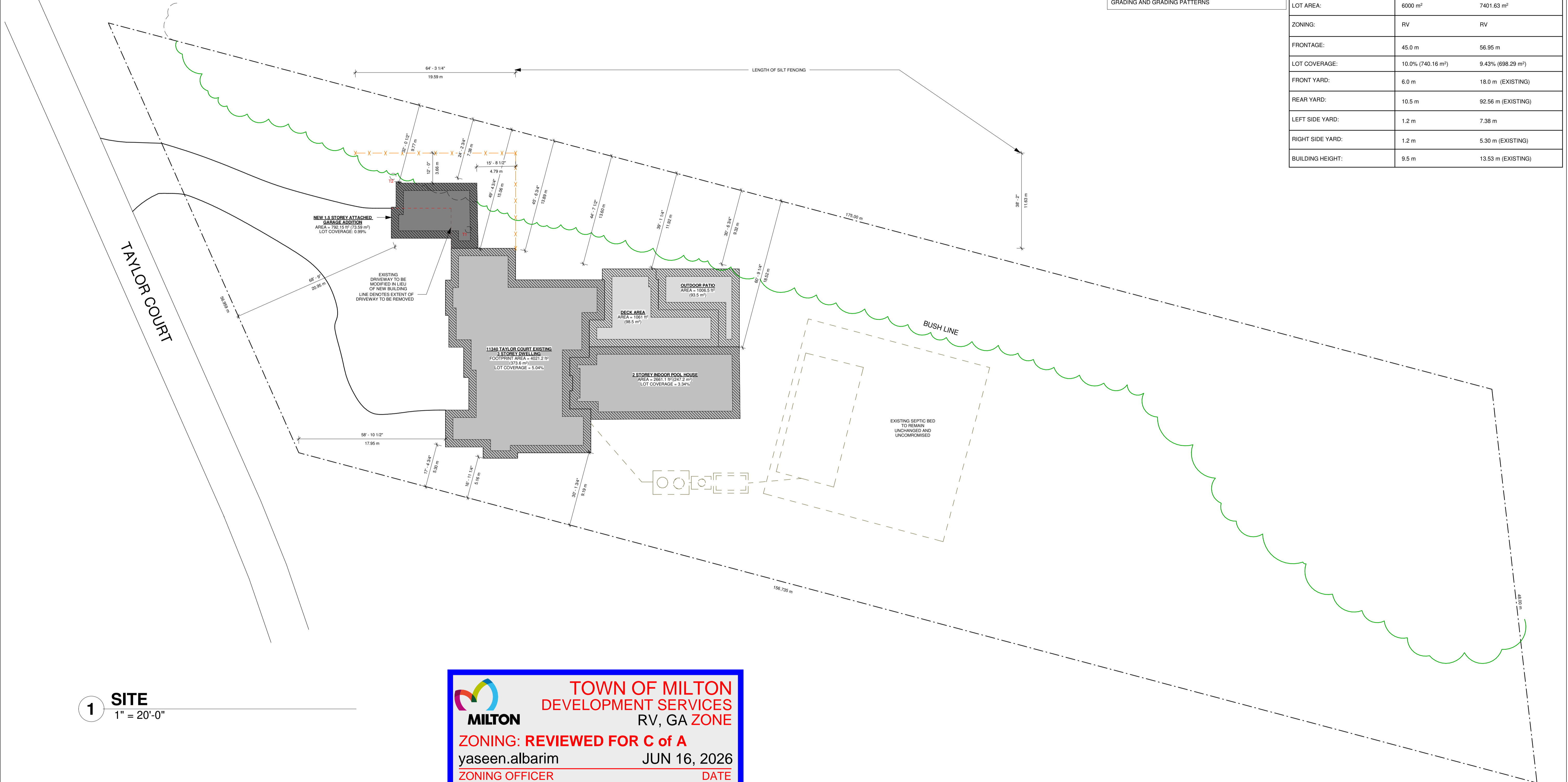


NOTE:
- DOWNSPOUT LOCATIONS ARE DRAIN AS PER EXISTING SITE CONDITIONS
- CONSTRUCTION OF THE PROPOSED ACCESSORY BUILDING IS NOT TO ADVERSELY AFFECT THE DRAINAGE PATTERNS

NOTE:
ALL DIMENSIONS ARE TAKEN FROM GIS MAPPING AND HAVE NOT BEEN VERIFIED IN ANY WAY. BY ACCEPTING THESE DRAWINGS THE HOMEOWNER AND CONTRACTOR AGREE WITH THEIR ACCURACY

NOTE:
NEW BUILDING IS NOT TO ADVERSELY AFFECT EXISTING GRADING AND GRADING PATTERNS

SITE INFORMATION		
ADDRESS:	11340 TAYLOR COURT, MILTON, ON.	
OWNER:	MR. & MRS. KENTH	
DESCRIPTION:	PART OF LOT 5 REGISTERED PLAN 20M-984 TOWN OF MILTON, REGIONAL MUNICIPALITY OF HALTON	
	REQUIRED (MAX):	PROVIDED:
LOT AREA:	6000 m ²	7401.63 m ²
ZONING:	RV	RV
FRONTAGE:	45.0 m	56.95 m
LOT COVERAGE:	10.0% (740.16 m ²)	9.43% (698.29 m ²)
FRONT YARD:	6.0 m	18.0 m (EXISTING)
REAR YARD:	10.5 m	92.56 m (EXISTING)
LEFT SIDE YARD:	1.2 m	7.38 m
RIGHT SIDE YARD:	1.2 m	5.30 m (EXISTING)
BUILDING HEIGHT:	9.5 m	13.53 m (EXISTING)



1 SITE
1" = 20'-0"

MILTON TOWN OF MILTON
DEVELOPMENT SERVICES
RV, GA ZONE

ZONING: REVIEWED FOR C of A
yaseen.albarim JUN 16, 2026
ZONING OFFICER DATE



ONTARIO OUTBUILDING

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WWW.ONTOUTBUILDING.COM
JESSE@ONTOUTBUILDING.COM | 905-746-5874

NEW ATTACHED GARAGE ADDITION

KENTH RESIDENCE
11340 TAYLOR COURT
MILTON ON. L0P 1B0

SITE PLAN

PROJECT NO: Project Number
LATEST REVISION: 2026-06-16
DRAWN BY: KV
CHECKED BY: Checker

SCALE: As indicated

A1

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2025-06-16 2:32:29 PM



The Corporation of the Town of Milton

Report To: Committee of Adjustment and Consent

From: Olivia Hayes

Date: June 25, 2026

File No: A26-030M

Subject: 387 Pine Street

Recommendation: THAT the application for minor variance **BE APPROVED SUBJECT TO THE FOLLOWING CONDITION:**

1. THAT the development shall be constructed generally in accordance with the site plan prepared by Matthews Design & Drafting Services Inc. and dated and stamped by Town Zoning on May 21, 2026.
2. THAT tree protection fencing, as shown in the approved site plan, shall be installed prior to construction and maintained for the duration of construction.
3. THAT a detailed grading plan shall be prepared to the satisfaction of Development Engineering staff, and submitted prior to issuance of a Building Permit.
4. THAT a stormwater management plan shall be prepared to the satisfaction of Development Engineering staff, and submitted prior to issuance of a Building Permit.
5. THAT a Building Permit be obtained within two (2) years from the date of the decision.
6. THAT the approval be subject to an expiry of two (2) years from the date of decision if the conditions are not met, if the proposed development does not proceed and/or a building permit is not secured.

General Description of Application

Under Section 45(1) of the Planning Act, a minor variance to Zoning By-law 016-2014, as amended, has been requested to:

- Permit accessory buildings/structures to have a maximum gross floor area (GFA) of 22.67 square metres, whereas a maximum total GFA of 14 square metres is allowed, representing an increase of 8.67 metres.

General Description of Application

- Permit a lot coverage of 29.02%, whereas a maximum lot coverage of 25% is allowed for a property with a lot area between 660 and 830 square metres, representing an increase of 4.02%
- Permit a garage to be set back a minimum of 3.85 metres from the building front wall, whereas a minimum setback of 5.5 metres is required, representing a decrease of 1.65 metres.

The Subject Property, known municipally as 387 Pine Street, is located at the northeast intersection of Prince Street and Pine Street in Milton's downtown. Surrounding uses are primarily low-rise, low-density residential, with retail uses to the north along Main Street East. Currently, the Subject Property is occupied by a one-and-a-half-storey residential dwelling, along with two accessory structures. The applicant is seeking to renovate the existing dwelling and construct a two-storey addition. To facilitate the proposed design, minor variances are required for additional lot coverage and a reduced garage setback. In addition, the application seeks to legalize the total GFA of two existing accessory structures.

Official Plan Designation (including any applicable Secondary Plan designations)

Halton Region Official Plan

Per Map 1 - Regional Structure, the Subject Property is designated Urban Area. Lands within the Urban Area designation are intended to accommodate the majority of the Region's residential and employment growth.

Town of Milton Official Plan

The Subject Property is designated Central Business District per Schedule A - Urban Area Land Use Plan. The Subject Property is further designated Low-Density Residential Sub-Area by Schedule B - Central Business District Land Use Plan. In addition, the Subject Property is listed on the Town's Heritage Register, and is subject to a Mature Neighbourhood Area overlay.

Within the Low-Density Residential Sub-Area, policy 8.5.3.25 directs that permitted uses shall be single detached, semi-detached, duplex dwellings, and additional residential units in accordance with other relevant policies of the Official Plan.

Policy 8.2.5.1 states that proposed development in the form of new housing, replacement housing, and additions and alterations within Mature Neighbourhood Areas

Official Plan Designation (including any applicable Secondary Plan designations)

may be permitted provided they are compatible and respectful of the character of the neighbourhood by incorporating scale, massing, building height, and other characteristics that are prevalent in the Mature Neighbourhood Area.

Policy 8.2.5.2 states that proposed development should be generally consistent with the setbacks, orientation and building separation distances within the Mature Neighbourhood Area, and that landscaping and fencing are encouraged to maintain established aesthetics and privacy.

Regarding the Subject Property's Heritage Listing, policy 3.3.1.10(b) encourages development proponents to protect cultural heritage resources by incorporating in any reconstruction or alterations, design features that are in harmony with the area's character and existing buildings in mass, height, setback and architectural details.

It is Staff's opinion that the proposal maintains the general intent of the Halton Region Official Plan and the Town of Milton Official Plan.

Zoning

The Subject Property is zoned Low Density Residential 1 (RLD1), with a Mature Neighbourhoods Area overlay, per Zoning By-law 016-2014. The RLD1 zone permits select low-rise residential uses, including detached, duplex, and semi-detached dwellings. Accessory structures are also permitted within this zone.

Variance One: Section 4.2.1, Table 4A(I) states that for a property with a lot area between 660 and 830 square metres, accessory structures are permitted a maximum total GFA of 14 square metres. The applicant is seeking to permit a maximum a total GFA of 22.67 square metres, representing an increase of 8.67 square metres.

Variance Two: Section 6.2, Table 6B(I) states that for a property having a lot area between 660 and 830 square metres, the maximum permitted lot coverage is 25%. The applicant is seeking to permit a maximum lot coverage of 29.02%, representing an increase of 4.02%.

Variance Three: Section 6.3.2(ii)(b) of the Zoning By-law states that where a garage face has a width greater than 3.5 metres, the garage face shall be set back a minimum of 5.5 metres from the building front wall. The applicant is seeking to permit a setback of 3.85 metres to the garage face, representing a decrease of 1.65 metres.

Consultation

Public Consultation

Notice for the hearing was provided pursuant to the Planning Act on June 9, 2026. As of the writing of this report on June 17, 2026, staff have not received any comments from members of the public.

Agency Consultation

No objections were filed with respect to the variance application from Town staff or external agencies.

Development Services Comments

The application seeks to permit extensive renovations to the existing 1.5-storey dwelling and introduce a 2-storey addition. Further, the application also seeks to legalize the GFA of two existing accessory structures. Planning staff have no concerns with the application, and do not anticipate adverse impacts on adjacent properties.

Variance One: Total Accessory Structure GFA

The intent of this provision of the Zoning By-law is to ensure that accessory structures remain clearly secondary and subordinate to the primary dwelling. Both accessory structures achieve this intent given their limited area, height, and location in the rear yard. Since the total GFA is split between two small structures, massing or visual impacts are negligible; any impacts can be mitigated by existing fencing and vegetation. Given the lack of adverse impacts, the proposed variance is minor. It is also desirable and appropriate, as the existing accessory structures facilitate storage and complement the primary residential function of the Subject Property.

Variance Two: Lot Coverage

The intent of this Zoning By-law provision is to prevent overdevelopment of a property, to preserve soft landscaping, and to avoid negative visual or massing impacts. While the application seeks a minor increase to the permitted lot coverage, the proposed development remains proportional to the area of the Subject Property, and exceeds the minimum required percentage of soft landscaping. Minimum setbacks are also maintained. Planning staff are satisfied that the proposed increase is minor and will not generate negative visual or massing impacts.

Variance Three: Garage Setback



Consultation

The intent of this Zoning By-law provision is to ensure habitable space, rather than garages, remain the most visually prominent feature in new development. Despite the variance sought, the proposed garage setback is sufficient to maintain this intent. Planning staff also note that as the Subject Property is a corner lot, its western elevation will act as a second frontage, enhancing the visual prominence of livable space in relation to the street. Substantial articulation and high-quality design of the proposed dwelling will also maintain the intent of the Zoning By-law and support the visual prominence of habitable space. Given the lack of adverse impacts, planning staff are satisfied that the proposed variance is minor. Moreover, the proposed garage setback is desirable as it will facilitate a cohesive floorplan connecting the existing dwelling to the addition.

As a whole, the application maintains the general intent of the Official Plan. Specifically, the proposal implements policies applicable to Mature Neighbourhood Areas by maintaining characteristics prevalent in the surrounding neighbourhood, such as height and building separation. In addition, the renovations will retain the footprint and general massing of the existing Heritage-listed dwelling, responding to policy 3.3.1.10(b).

In sum, staff are of the opinion that the application is minor in nature, maintains the intent of the Zoning By-law and Official Plan, and is desirable for the appropriate development or use of the land, building or structure.

Respectfully submitted,

Olivia Hayes, B.E.S.
Planner, Development Review

For questions, please contact: Olivia.Hayes@Milton.ca Phone: Ext. 2454

Attachments

Figure 1 - Site Plan
Figure 2 - Floorplans and Elevations

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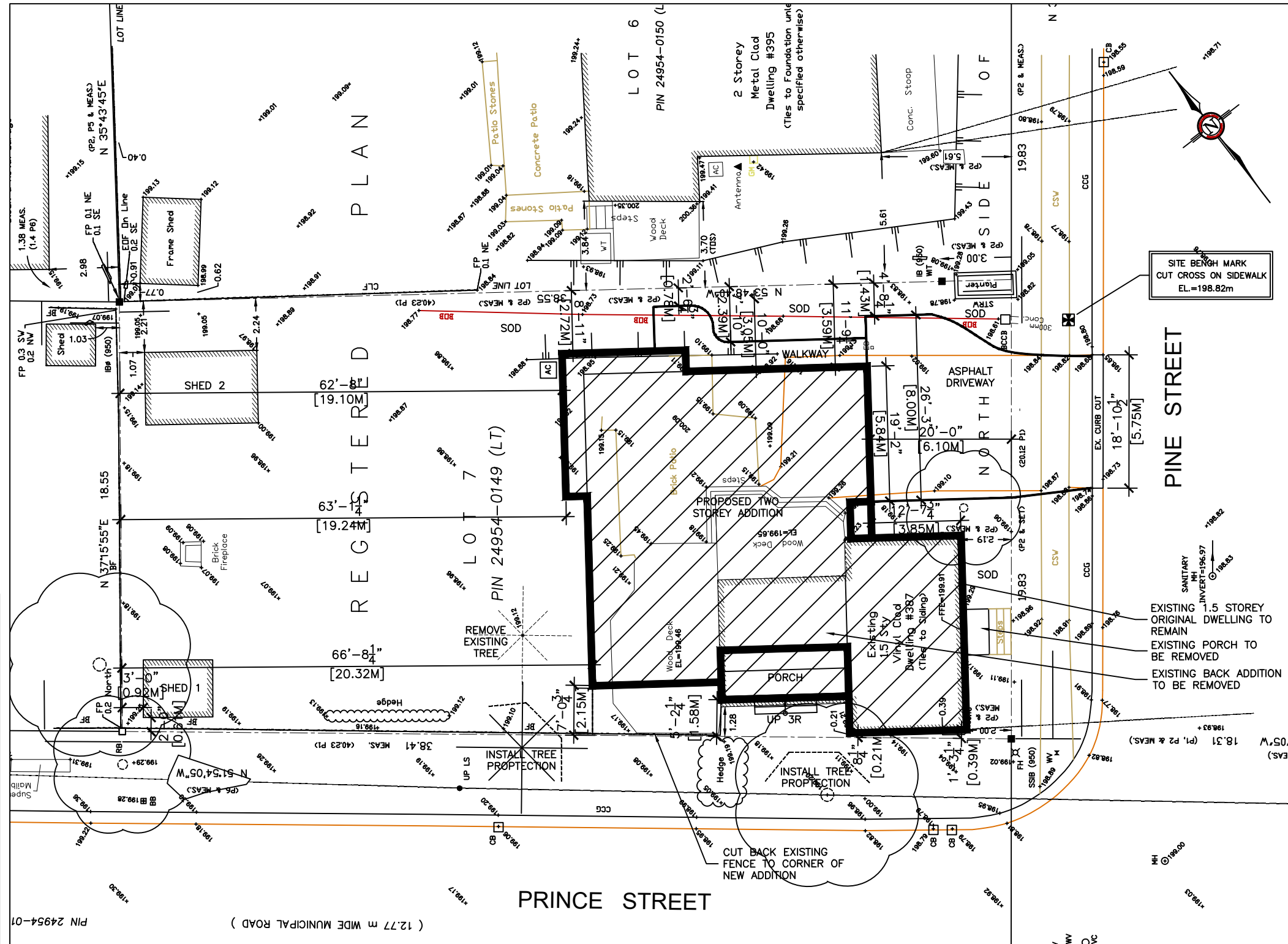
MILTON TOWN OF MILTON DEVELOPMENT SERVICES RLD1 ZONE
ZONING: REVIEWED FOR C of A
 rob.catarino MAY 21, 2026
 ZONING OFFICER DATE

Information for this Site Plan taken from Survey of Lot 7, Block 15, REG. Plan 9 Town of Milton, Regional Municipality of Halton dated August 31, 2023 by Cunningham McConnell Ltd. Surveyors

	HEIGHT
Site Area = 7946.49 sq.ft. (738.25m ²) (0.074 H)	6.88m
Existing House Footprint = 782.08 sq.ft. (72.66m ²)	
Existing To Be Demolished = 326.25 sq.ft. (30.31m ²)	
Proposed Overall Footprint = 2305.89 sq.ft. (214.22m ²)	9.27m
Existing Porch Removed = 24.82 sq.ft. (2.31m ²)	
Existing Shed 1 = 80.08 sq.ft. (7.44m ²)	2.50m
Existing Shed 2 = 163.89 sq.ft. (15.23m ²)	2.99m
Existing Deck Removed = 570.77 sq.ft. (53.03m ²)	(MIDPOINT)
Proposed Main Floor = 1238.61 sq.ft. (115.07m ²)	
Proposed Garage = 480.82 sq.ft. (44.67m ²)	
Proposed Second Floor = 1524.37 sq.ft. (141.62m ²)	
Proposed Front Porch = 126.00 sq.ft. (11.71m ²)	
Proposed Side Porch = 20.67 sq.ft. (1.92m ²)	
Proposed Balcony = 73.30 sq.ft. (6.81m ²)	
Proposed Renovations = 451.05 sq.ft. (41.90m ²)	
Dwelling Lot Coverage = 29.02%	

2 Site Statistics

MV1 Scale: N.T.S.



1 Site Plan

MV1 Scale: 1:200

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Revised For Minor Variance	May 20/26
Issued For Minor Variance	May 4/26
Issued/Revisions	date

Client No.	
Project No.	2313

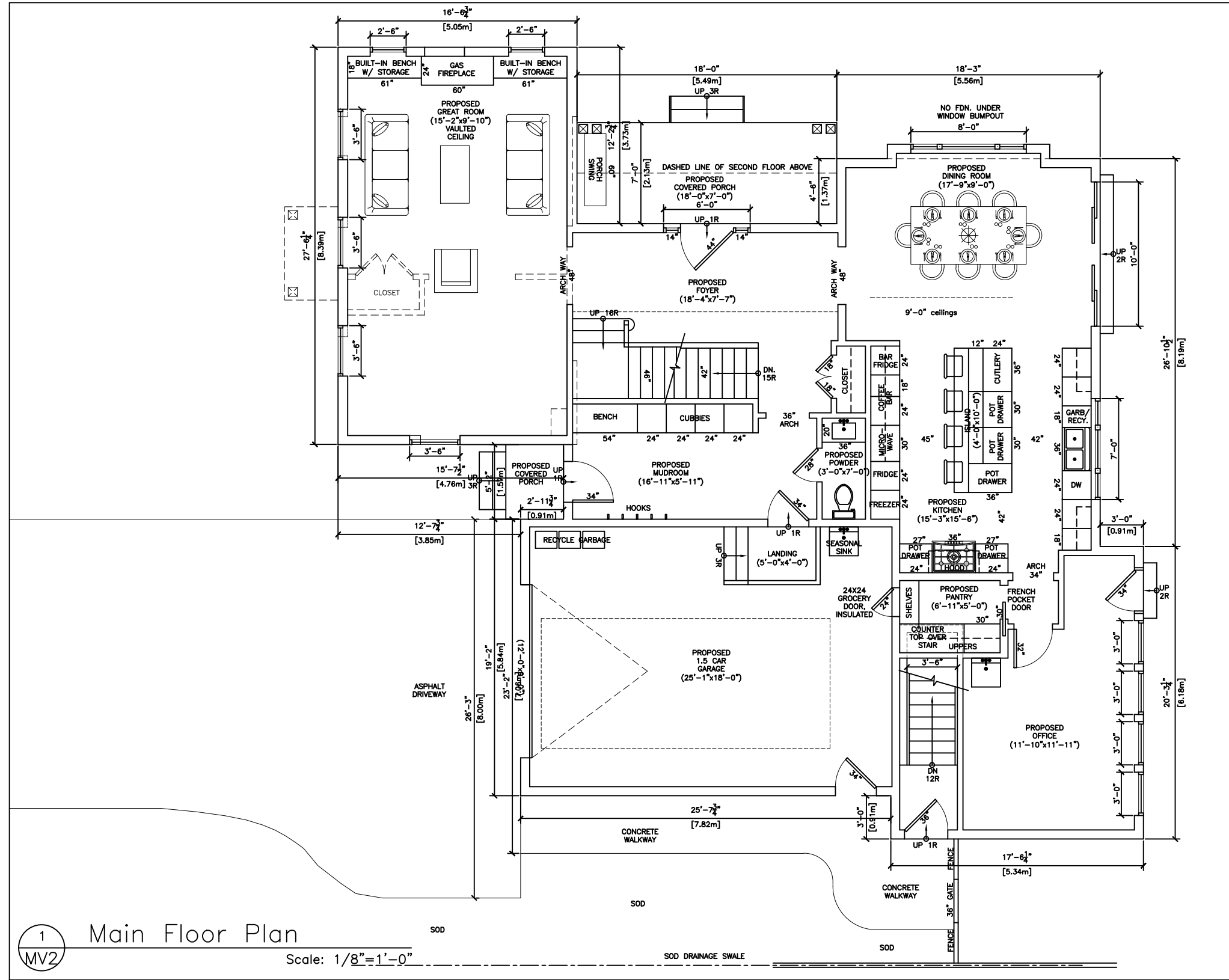
Dwg. Title:	Site Plan and Site Statistics
scale:	As Noted
date:	Feb 2023
drawn by:	D.M./M.K.
Plotted:	May 2026
reviewed by:	D.M.

Project:	Theriault/Puga Residence
	387 Pine Street Milton, Ontario L9T 1L3

dwg. no.	MV-1
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1
MV2

Main Floor Plan

Scale: 1/8" = 1'-0"



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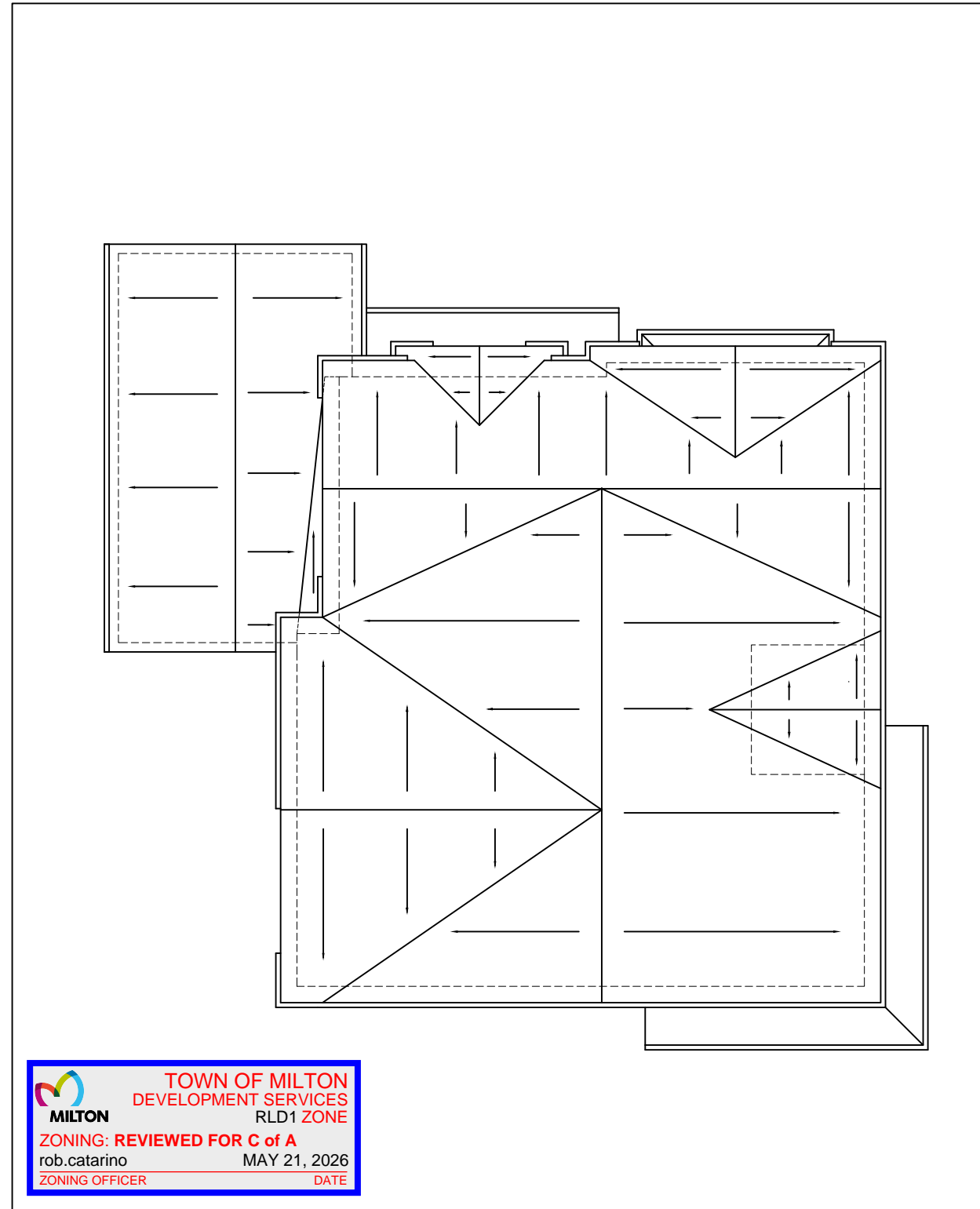
Client No.	
Project No.	2313

Dwg. Title:
Main Floor Plan
 scale: As Noted Plotted: May 2026
 date: Feb 2023
 drawn by: D.M./M.K. reviewed by: D.M.

Project:
Theriault/Puga Residence
 387 Pine Street
 Milton, Ontario
 L9T 1L3

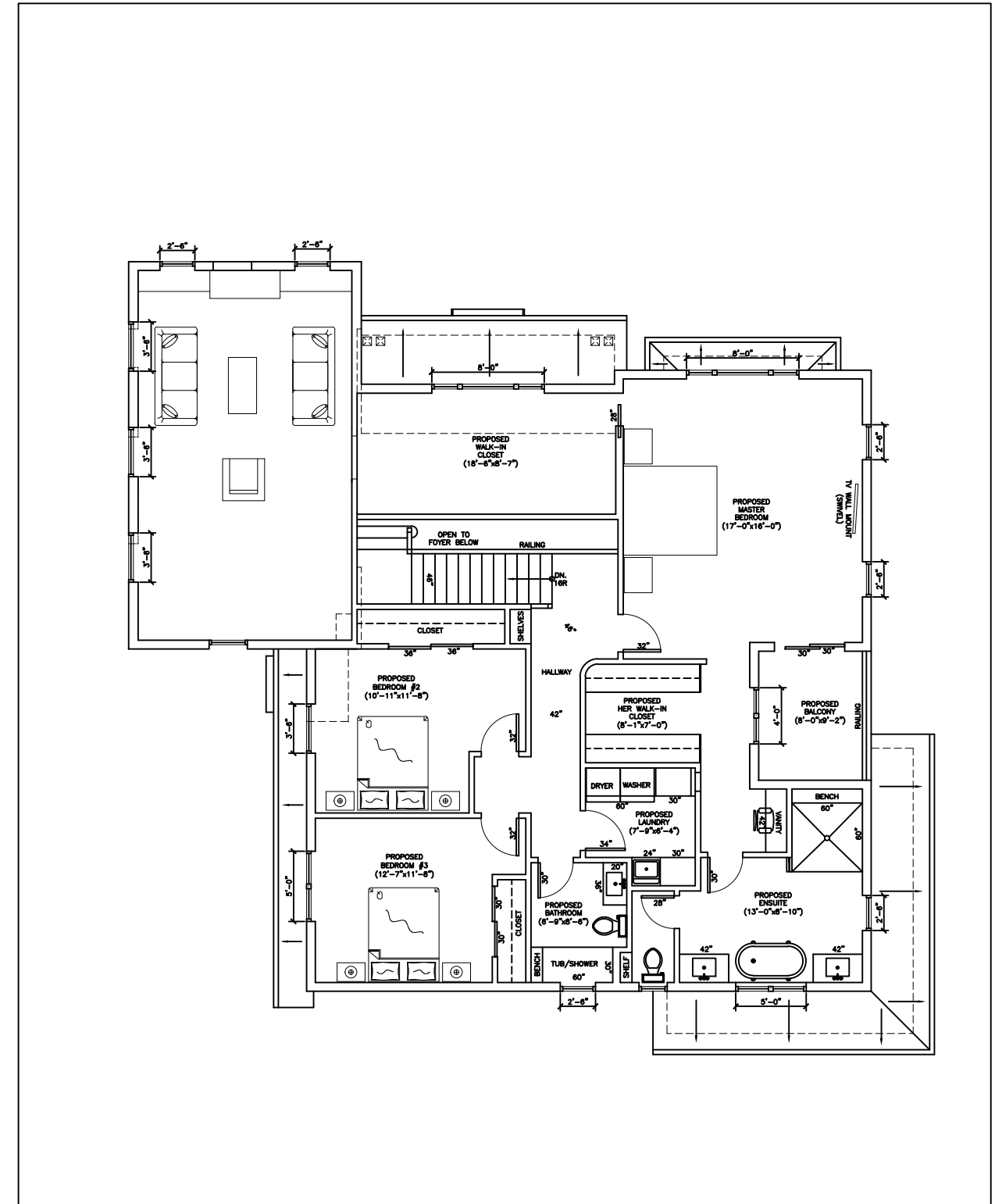
dwg. no.
MV-2

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TOWN OF MILTON
 DEVELOPMENT SERVICES
 RLD1 ZONE
ZONING: REVIEWED FOR C of A
 rob.catarino MAY 21, 2026
 ZONING OFFICER DATE

1 Roof Plan
 MV3 Scale: 3/32"=1'-0"



1 Second Floor Plan
 MV3 Scale: 3/32"=1'-0"



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Issued For Minor Variance	May 4/26
Issued/Revisions	date

Client No.	
Project No.	2313

Dwg. Title:	Second Floor Plan & Roof Plan
scale:	As Noted
date:	Feb 2023
drawn by:	D.M./M.K.
Plotted:	May 2026
reviewed by:	D.M.

Project:	Theriault/Puga Residence
	387 Pine Street
	Milton, Ontario
	L9T 1L3

dwg. no.	MV-3
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1 Front South Elevation
 MV4 Scale: 1/8"=1'-0"



TOWN OF MILTON
 DEVELOPMENT SERVICES
 RLD1 ZONE
 ZONING: REVIEWED FOR C of A
 rob.catarino MAY 21, 2026
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2 West Side Elevation
 MV4 Scale: 1/8"=1'-0"

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Issued For Minor Variance	May 4/26
Issued/Revisions	date

Client No.	
Project No.	2313

Dwg. Title:	South and West Elevations (Pine and Prince Street Views)
scale:	As Noted
date:	Feb 2023
drawn by:	D.M./M.K.
Plotted:	May 2026
reviewed by:	D.M.

Project:	Theriault/Puga Residence
	387 Pine Street Milton, Ontario L9T 1L3

dwg. no.	MV-4
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1 Back North Elevation
 MV5 Scale: 1/8"=1'-0"



2 East Side Elevation
 MV5 Scale: 1/8"=1'-0"



MILTON
 TOWN OF MILTON
 DEVELOPMENT SERVICES
 RLD1 ZONE
 ZONING: REVIEWED FOR C of A
 rob.catarino MAY 21, 2026
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Issued For Minor Variance	May 4/26
Issued/Revisions	date

Client No.	
Project No.	2313

Dwg. Title:
 North and East Elevations
 scale: As Noted Plotted: May 2026
 date: Feb 2023
 drawn by: D.M./M.K. reviewed by: D.M.

Project:
 Theriault/Puga Residence
 387 Pine Street
 Milton, Ontario
 L9T 1L3

dwg. no.
 MV-5



The Corporation of the Town of Milton

Report To: Committee of Adjustment and Consent

From: Olivia Hayes

Date: June 25, 2026

File No: A26-031M

Subject: 171 Bronte Street South

Recommendation: THAT the application for minor variance **BE APPROVED SUBJECT TO THE FOLLOWING CONDITIONS:**

1. THAT the development shall be constructed generally in accordance with the site plan prepared by Jansen Consulting and dated and stamped by Town Zoning on June 2, 2026.
2. THAT a Road Occupancy permit be obtained prior to issuance of a Building Permit.
3. THAT a Curb Cut and Entrance permit be obtained prior to issuance of a Building Permit.
4. THAT a Building Permit be obtained within two (2) years from the date of the decision.
5. THAT the approval be subject to an expiry of two (2) years from the date of decision if the conditions are not met, if the proposed development does not proceed and/or a building permit is not secured.

General Description of Application

Under Section 45(1) of the Planning Act, the following minor variance to Zoning By-law 016-2014, as amended, has been requested to:

- Permit a deck associated with an accessory building containing an additional dwelling unit to be set back a minimum of 0.2 metres from the interior side lot line, whereas a setback of 1.2 metres is required, representing a decrease of 1 metre

The Subject Property, known municipally as 171 Bronte Street South, is located north of Barton Street, on the eastern side of Bronte Street. Surrounding uses include low-rise, mid-rise, and high-rise residential. Currently, the Subject Property is occupied by a one-storey detached dwelling, with an accessory structure in the rear yard. The applicant is looking to convert this accessory structure into an additional residential unit (ARU). To facilitate the proposal, a minor variance is needed to legalize the setback of the existing

General Description of Application

deck attached to the accessory structure. Planning staff have no concerns with the application.

Official Plan Designation (including any applicable Secondary Plan designations)

Halton Region Official Plan

Per Map 1 - Regional Structure, the Subject Property is designated Urban Area. The Urban Area designation is intended to accommodate the majority of the Region's residential and employment growth.

Town of Milton Official Plan

The Subject Property is designated Residential Area per Schedule A - Urban Area Land Use Plan, with a Mature Neighbourhood Area overlay.

Policy 8.2.1.1 states that the purpose of the Residential Area designation is to accommodate a mix of low, medium and high -density residential development.

Within the Mature Neighbourhood Areas, policy 8.2.5.1 states that proposed development in the form of new housing, replacement housing, and additions and alterations within Mature Neighbourhood Areas may be permitted provided they are compatible and respectful of the character of the neighbourhood by incorporating scale, massing, building height, and other characteristics that are prevalent in the Mature Neighbourhood Area.

Policy 8.2.5.2 states that proposed development should be generally consistent with the setbacks, orientation and building separation distances within the Mature Neighbourhood Area. Landscaping and fencing are encouraged to maintain established aesthetics and privacy.

With respect to ARUs, policy 7.1.4 states that ARUs shall be permitted either within a detached, semi-detached or townhouse dwelling, or within an ancillary building or structure on the same lot as a detached, semi-detached or townhouse dwelling, subject to conformity with the following:

- a. an ARU shall not be located on lands identified as hazard lands or as being within the regulatory flood plain, unless where specifically permitted by the Conservation Authority;

Official Plan Designation (including any applicable Secondary Plan designations)

- g. Where municipal water services and/or municipal sewage services are available to a lot, an ARU must be connected to any available municipal water services and/or municipal sewage services;
- k. An ARU must have no adverse effect on stormwater management systems;
- l. An ARU must have no adverse effect on site drainage as demonstrated through a grading plan;
- m. Safe access to an ARU must be ensured by meeting fire and emergency service requirements;
- n. Severance of an ARU from the lot shall not be permitted; and
- o. An ARU shall be registered with the Town in accordance with the provisions of the Municipal Act.

It is Staff's opinion that the proposal maintains the general intent and purpose of the Halton Region Official Plan and the Town of Milton Official Plan.

Zoning

The Subject Property is zoned Low Density Residential 1 (RLD1) by By-law 014-2016, with a Mature Neighbourhood Overlay. Certain low-density residential uses, including detached, duplex, semi-detached dwellings, and ARUs, are permitted within the RLD1 zone. Accessory structures are also permitted.

Variance One: Section 4.3.1(iii) states that notwithstanding any provisions of Section 4.3.1 to the contrary, decks associated with an accessory building containing an additional dwelling unit shall be subject to the minimum setbacks required for the accessory building.

Section 4.2.5(iii) of the Zoning By-law states that an accessory building containing an ARU shall be located no closer than 1.2 metres from an interior side lot line. The applicant is seeking to permit a deck associated with the ARU to have a minimum setback of 0.2 metres, representing a decrease of 1 metre.

Consultation

Public Consultation

Notice for the hearing was provided pursuant to the Planning Act on June 9, 2026. As of the writing of this report on June 18, 2026, staff have not received any comments from members of the public.

Agency Consultation

Consultation

No objections were filed with respect to the variance application from Town staff or external agencies. Engineering staff have noted that a Road Occupancy permit and a Curb Cut and Entrance permit will be required prior to construction.

Development Services Comments

The application seeks to facilitate the conversion of an existing accessory structure into a studio-sized ARU. Due to the deficient setback of an existing deck attached to the accessory structure, a minor variance is required. Planning staff have no concerns with the application, and do not anticipate adverse impacts on adjacent properties.

The purpose of this Zoning By-law provision is to ensure ARUs located in accessory structures, and any associated decks, do not generate noise or privacy concerns for neighbouring properties. The existing deck is small in size, limiting its function as an amenity area, and is very low to the ground (with a height of less than 0.3 metres). There is also a row of mature trees along the interior lot line of the Subject Property, forming a visual buffer. As such, planning staff are satisfied that the existing deck will not cause noise or privacy concerns, and that the variance is minor.

Planning staff also note the uniquely deep rear lots of the Subject Property and abutting parcels. With a rear yard depth exceeding 24 metres, the existing deck will have no impact on the majority of the rear yard of the abutting property to the west. As such, the proposed variance should not interfere with the use and enjoyment of the adjacent lot.

With respect to Official Plan policies, the application will have no impact on the ability of the ARU to satisfy the criteria of policy 7.1.4. In particular, Engineering staff did not flag any drainage concerns with the application. Moreover, the proposed ARU will remain compatible with existing characteristics of the surrounding area, and incorporates landscaping to maintain privacy, responding to the policy directives for Mature Neighbourhood Areas.

The proposed variance is desirable as it will permit the introduction of an ARU within Milton's established urban area, facilitating the use of an existing accessory structure. Additionally, the applicant has worked with planning staff to revise the application, reducing the number of variances required and ensuring the proposal satisfies all other zoning requirements that pertain to ARUs.



Consultation

In sum, planning staff are of the opinion that the application is minor in nature, maintains the intent of the Zoning By-law and Official Plan, and is desirable for the appropriate development or use of the land, building or structure.

Respectfully submitted,

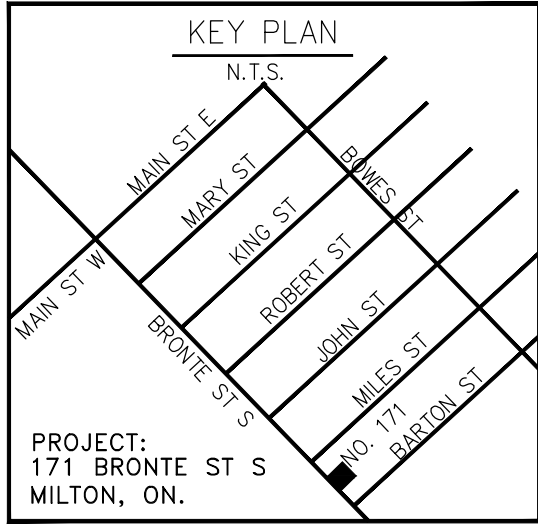
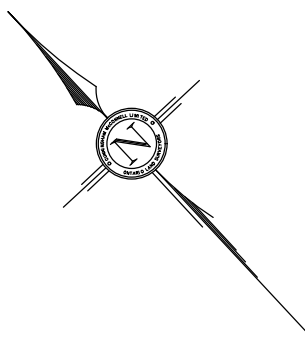
Olivia Hayes, B.E.S.
Planner, Development Review

For questions, please contact: Olivia.Hayes@Milton.ca Phone: Ext. 2454

Attachments

- Figure 1 - Site Plan
- Figure 2 - ARU Architectural Drawings

SITE PLAN OF 171 BRONTE ST S



LEGEND

▶ ENTRY POINTS INTO BUILDING

EXISTING GRADES TO
REMAIN UNLESS NOTED
EXISTING DRAINAGE
PATTERN TO REMAIN

FILL MATERIAL & TOPSOIL
TO BE REMOVED OFF SITE
CLEAN BACKFILL MATERIAL
TO BE RETURNED TO SITE
FOR BACKFILL

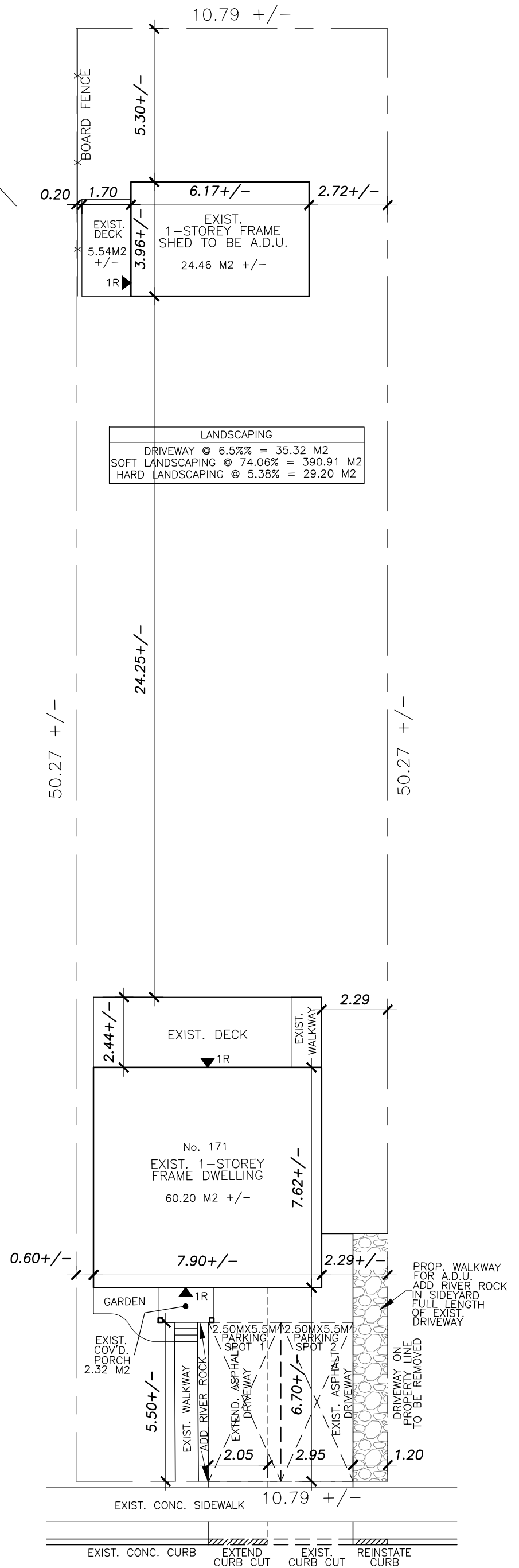
TOWN OF MILTON NOTES:

- PRIOR TO THE COMMENCEMENT OF ANY WORKS ON THE SITE, SNOW FENCE IS INSTALLED ON THE PERIMETER OF THE PROPERTY AND AT LOCATIONS AS DETERMINED BY THE MANAGER, DEVELOPMENT ENGINEERING, AND THAT THE SNOW FENCE SHALL REMAIN IN PLACE UNTIL SUCH TIME AS OTHERWISE DIRECTED BY THE MANAGER, DEVELOPMENT ENGINEERING.
- SILT CONTROLS ARE TO BE IN PLACE PRIOR TO THE START OF SITE WORKS, AND BE MAINTAINED FOR THE DURATION OF CONSTRUCTION.
- PRIOR TO COMMENCEMENT OF ANY WORKS WITHIN THE MUNICIPAL ROAD ALLOWANCE, THE OWNER IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE ENGINEERING SERVICES DEPARTMENT, TOWN OF MILTON, FOR THE PURPOSES OF VEHICULAR ACCESS TO THE PROPERTY, (ENTRANCE PERMIT), AND FOR SERVICING EXCAVATIONS WITHIN THE MUNICIPAL ROAD ALLOWANCE, (ROAD OCCUPANCY PERMIT).
- SILT FENCE IS REQUIRED TO BE INSTALLED AROUND ALL DISTURBED AREAS AS PER OPSD 219.110
- ALL FILL SHALL BE COMPACTED TO 95% STANDARD PROCTOR DENSITY AND COMPACTION TESTING SHALL BE UNDERTAKEN TO THE SATISFACTION OF THE DIRECTOR, DEVELOPMENT ENGINEERING
- ALL DISTURBED AREAS WITHIN THE MUNICIPAL ROAD ALLOWANCE ARE TO BE REINSTATED TO TOWN STANDARDS
- ALL DISTURBED AREAS WITHIN THE MUNICIPAL ROAD ALLOWANCE ARE TO BE REINSTATED TO EXISTING CONDITIONS OR BETTER.



JANSEN CONSULTING PREPARED THIS SITE PLAN BASED OFF A DATED SITE PLAN PROVIDED BY THE OWNERS. JANSEN CONSULTING DOES NOT TAKE ANY RESPONSIBILITY FOR SET BACKS TO EXISTING OR PROPOSED BUILDINGS AS THE PROVIDED SITE PLAN WAS NOT BASED OFF AN ONTARIO LAND SURVEYOR.

METRIC DISTANCES AND/OR COORDINATES SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048.



BRONTE ST S

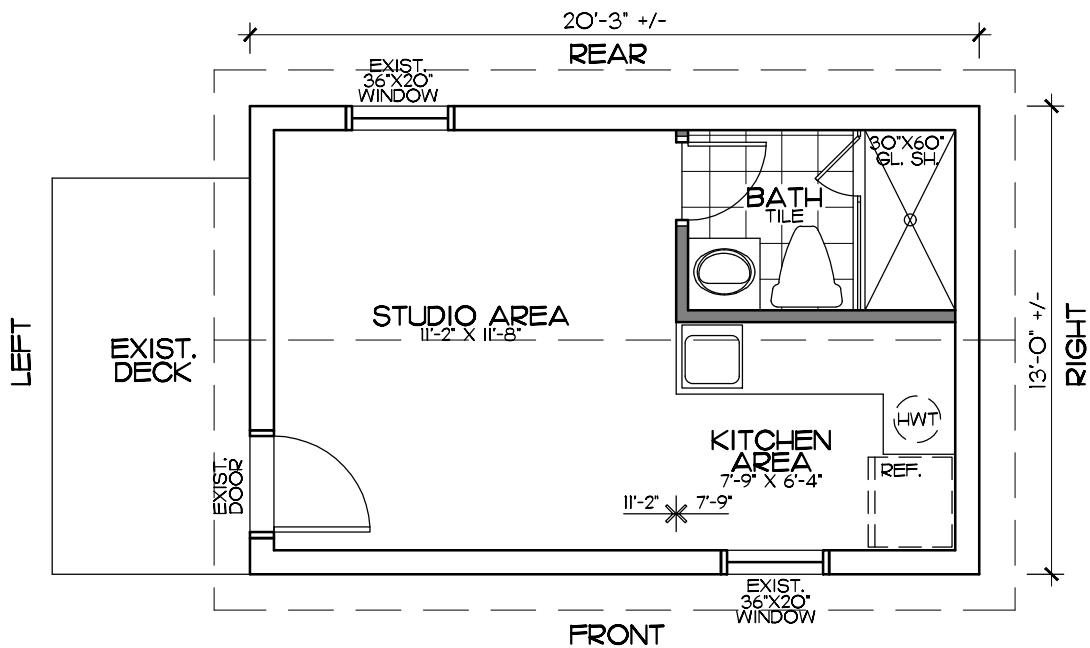
REVISIONS NO.	DATE	DESCRIPTION
1	MAY 27, 26	REVISIONS TO DRIVEWAY

QUALIFIED DESIGNER BCIN - 30272
FIRM BCIN - 110042
DATE: APR 30 26
SIGNATURE:

PROJECT 171 BRONTE ST S	DRAWING SITE PLAN
SCALE 1:50	DATE APR 30 26
MODEL TYPE M-008	
DRAWN BY AS	CHK'D BY AS
PROJECT LOCATION MILTON	PROJECT NO. 2026-008
FILE NO. 2026-008S	DRAWING NO. S1

OWNERS INFORMATION
MARK OLLERENSHAW


TOWN OF MILTON
 DEVELOPMENT SERVICES
 RLD1 ZONE
ZONING: REVIEWED FOR C of A
 sherri.jamieson MAY 27, 2026
 ZONING OFFICER DATE



STUDIO A.D.U MAIN FLOOR PLAN

= NEW WALLS
 = EXIST. WALLS

NOTE:
 8'-0" APPROX. HIGH CEILING
 THROUGHOUT GROUND FL.

MAIN FLOOR AREA = 263.25 S.F. (24.46 M2)

LOT AREA = 542.41 M2
LOT COVERAGE ALLOWED • 30% = 162.72 M2
EXISTING HOUSE LOT COVERAGE • 11.53% = 62.52 M2 (672.98 S.F.)
EXISTING SHED TO BE A.D.U. LOT COVERAGE • 4.5% = 24.46 M2 (263.25 S.F.)
TOTAL LOT COVERAGE • 16.03% = 86.98 M2 (936.23 S.F.)

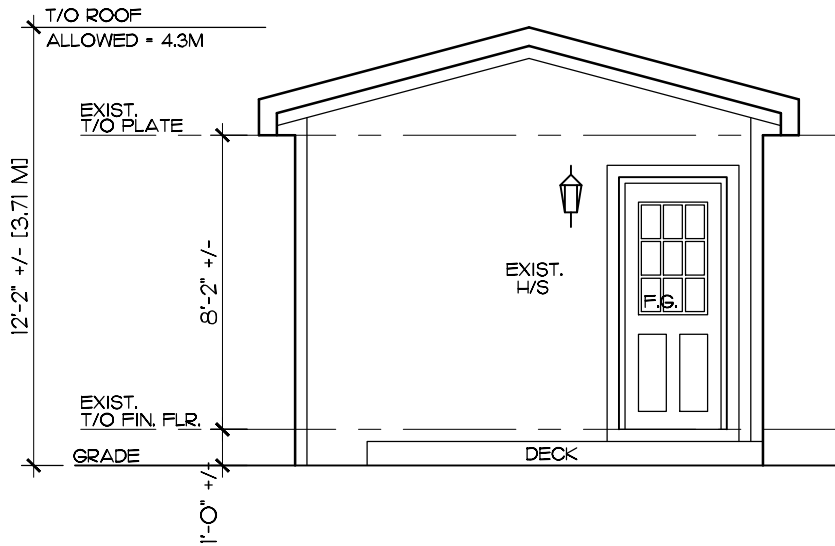
DESIGN DRAWINGS APPROVED BY: _____ INITIAL: _____ DATE: _____



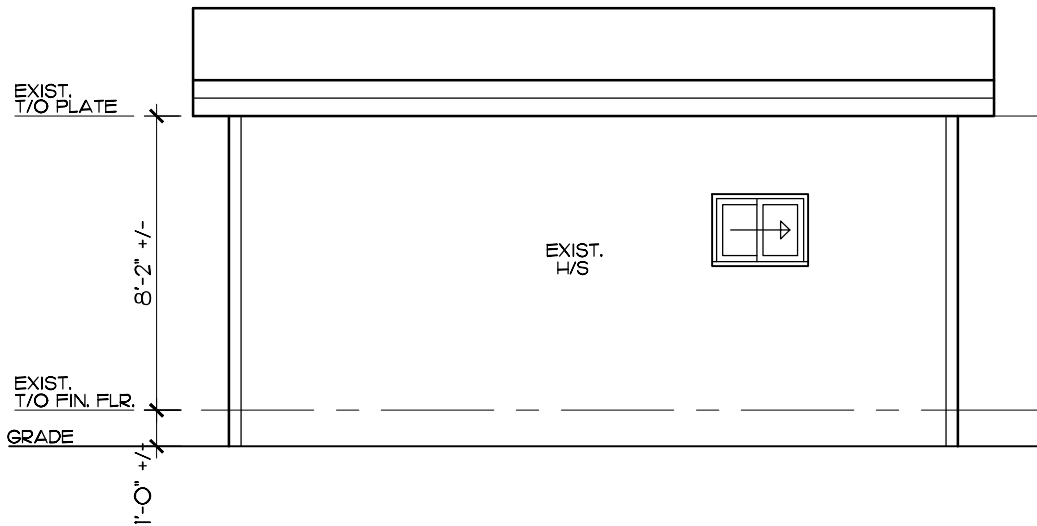
70 Main Street N., P.O. Box 38
 Campbellville, ON, L0P 1B0
 Ph. 905-854-9696
 Fax 905-854-9559
 Cell 905-815-3438
 EMAIL : jeffjansendesign@gmail.com

OWNERS INFORMATION :
 MARK OLLERENSHAW
 171 BRONTE ST S
 MILTON, ON

D01



**STUDIO A.D.U
LEFT ELEVATION**



**STUDIO A.D.U
FRONT ELEVATION**

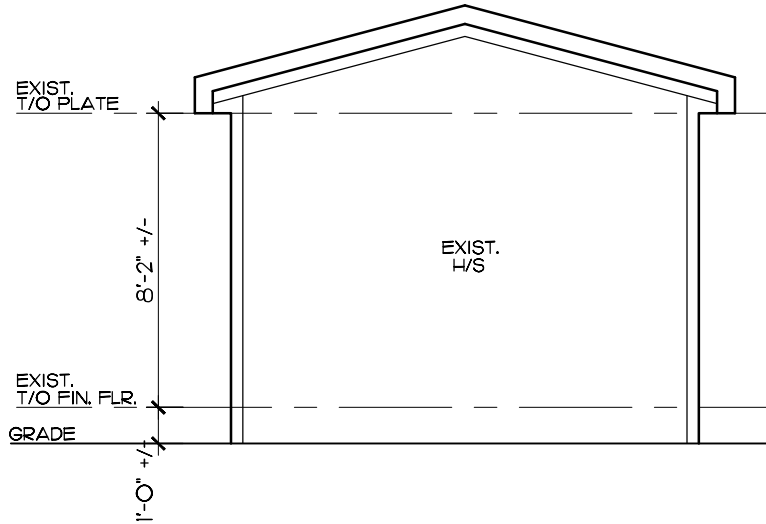
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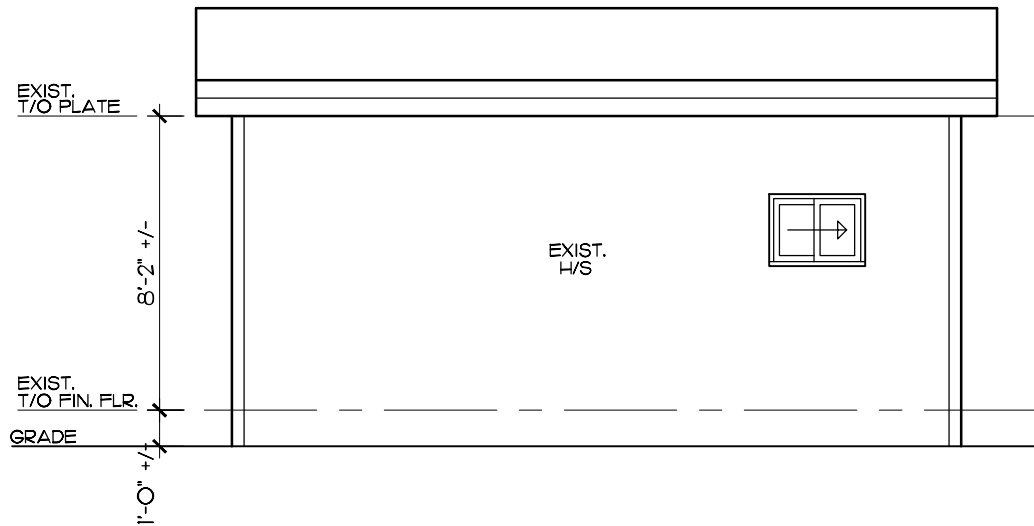
70 Main Street N., P.O. Box 38
 Campbellville, ON, L0P 1B0
 Ph. 905-854-9696
 Fax 905-854-9559
 Cell 905-815-3438
 EMAIL : jeffjansendesign@gmail.com

OWNERS INFORMATION :
 MARK OLLERENSHAW
 171 BRONTE ST S
 MILTON, ON

D02



**STUDIO A.D.U
RIGHT ELEVATION**



**STUDIO A.D.U
REAR ELEVATION**

DESIGN DRAWINGS APPROVED BY: _____ INITIAL: _____ DATE: _____



The Corporation of the Town of Milton

Report To: Committee of Adjustment and Consent

From: Taylor Wellings, MSc (PI), MCIP, RPP

Date: June 25, 2026

File No: A-26-026M

Subject: 280 Sydney Street

Recommendation: THAT the application for minor variance **BE APPROVED SUBJECT TO THE FOLLOWING CONDITIONS:**

1. That the development shall be constructed in accordance with the site plan prepared by BLDG Workshop and stamped by Town Zoning on May 15, 2026.
2. That a Building Permit be obtained within two (2) years from the date of the decision; and
3. That the approval be subject to an expiry of two (2) years from the date of decision if the conditions are not met, if the proposed development does not proceed and/or a building permit is not secured.

General Description of Application

Under Section 45(1) of the Planning Act, the following minor variance to Zoning By-law 016-2014, as amended, has been requested:

- To allow a minimum rear setback of 1.29 metres, a difference of - 0.21 metres;
- To allow the gross floor area to be 16.4% of the lot area of 163.5 sq metres, a difference of +6.4% and +53.5 sq metres;
- To allow the building height to be 6.91 metres, a difference of + 2.61 metres; and
- To allow for the existing driveway to remain in the rear yard despite not servicing a detached garage/carport.

The subject property is known municipally as 280 Sydney Street and located near the corner of Sydney Street and Commercial Street. The subject property contains a single-detached dwelling with a double-car detached garage. Surrounding uses are residential and primarily single-detached dwellings.

The minor variance application has come forward to convert the existing detached garage into an Additional Residential Unit (ARU) and to utilize the existing site conditions.

Official Plan Designation (including any applicable Secondary Plan designations)

The subject property is designated as Residential Area on Schedule A (Urban Land Use Plan) of the Town of Milton's Official Plan. This designation permits single-detached dwelling, accessory structures as well as ARUs.

It should be noted that the subject property is located within the Mature Neighbourhood Area. Section 8.2.5.1 of the Town's Official Plan states that proposed development in the form of new housing, replacement housing, and additions and alterations within Mature Neighbourhood Areas may be permitted provided they are compatible and respectful of the character of the neighbourhood by incorporating scale, massing, building height, and other characteristics that are prevalent in the Mature Neighbourhood Area.

Given that the proposed ARU will be utilizing an existing detached garage, Planning staff are of the opinion that there is no negative impact to the character of the property or surrounding area. Further, the existing detached garage

Section 7.1.4.1 of the Town's Official Plan states that Additional Residential Units (ARUs) shall be permitted either within an ancillary building or structure on the same lot as a detached, semi-detached or townhouse dwelling, subject to conformity with the following:

1. An ARU shall not be located on lands identified as hazard lands or as being within the regulatory flood plain, unless where specifically permitted by the Conservation Authority;
2. Where municipal water services and/or municipal sewage services are available to a lot, an ARU must be connected to any available municipal water services and/or municipal sewage services;
3. An ARU will be compatible with neighbouring properties and the surrounding neighbourhood or rural area by taking into consideration scale and built form;
4. An ARU must have no adverse effect on stormwater management systems;
5. An ARU must have no adverse effect on site drainage as demonstrated through a grading plan;
6. Safe access to an ARU must be ensured by meeting fire and emergency service requirements;
7. Severance of an ARU from the lot shall not be permitted; and
8. An ARU shall be registered with the Town in accordance with the provisions of the Municipal Act .

Given the ARU is proposed within an existing detached garage, the site is connected to municipal servicing, not within the Regulatory Flood Plain, is accessible to public transit, and subject to the approval of this application and Building Permit issuance, will comply with all applicable law, including the Zoning By-law and Ontario Building Code.

Official Plan Designation (including any applicable Secondary Plan designations)

Further, Section 3.1.1.11 of the Town's Official Plan states that it is the policy of the Town to increase the supply of ground-related and rental housing, allow flexibility for multi-generational living, and increase opportunities for affordable housing by permitting additional residential units (ARUs) in accordance with the policies of this Plan.

Given that the application proposes utilizing an existing detached garage and converting it into an ARU, Planning Staff are of the opinion that the proposed development is in conformity with the Town of Milton's Official Plan.

Zoning

The subject lands are zoned Residential Low Density 1 (RLD1) under the Town of Milton Zoning By-law 016-2014, as amended. The RLD1 zone permits a variety of residential uses, including single-detached dwellings, accessory structures as well as ARUs.

Section 4.2.5 of the Town's Zoning By-law provides the regulations for Accessory Buildings Containing an Additional Dwelling Unit. In addition to the requirements of Section 4.1, an accessory building, excluding a detached garage, that contains a permitted additional dwelling unit in accordance with Section 4.10, is permitted subject to the following provisions:

- i) the building shall be permitted in a rear yard or an interior side yard;
- ii) it shall be located no closer to an exterior side lot line than permitted for the principal building;
- iii) it shall be located no closer than 1.2 m from an interior side lot line;
- iv) it shall be located no closer than 1.5 m from a rear lot line;
- v) it shall be located no closer than 3.5 m from the principal building, including any structure with a roof that may be attached to the principal building, on the lot;
- vi) the gross floor area shall not exceed 10% of the lot area or 110 m², whichever is less;
- vii) the height shall not exceed: a)
 - a) 3.5 m in the case of a flat roof, measured from the established grade to the uppermost point of the roof surface or parapet, whichever is greater; or
 - b) 4.3 m in the case of a gable, hip, gambrel, or mansard roof, measured from the established grade to the uppermost point of the roof surface;
- viii) notwithstanding the definition of Lot Coverage in Section 3, the building shall be included in the lot coverage calculation; and,
- ix) roof-top amenity areas shall not be permitted.

Other than the requested variances noted, the proposed development meets the remaining regulations for Accessory Buildings Containing an Additional Dwelling Unit.

Variance One: Minimum Rear Yard Setback



Zoning

Section 4.2.5 iv) of the Town's Zoning By-law states that a minimum rear yard setback for a detached additional dwelling unit is 1.5 metres. The applicant is requesting permission to allow a minimum rear yard setback of 1.29 metres, a difference of - 0.21 metres, to facilitate the proposed development.

Variance Two: Size of ARU

Section 4.2.5 vi) of the Town's Zoning By-law states that the accessory building containing an additional dwelling unit shall not have a gross floor area that exceeds 10% of the lot area of 11 sq metres area. The applicant is requesting permission to allow the gross floor area to be 16.4% of the lot area of 163.5 sq metres, a difference of + 6.4% and + 53.5 sq metres, to facilitate the proposed development.

Variance Three: Building Height

Section 4.2.5 vii) b) of the Town's Zoning By-law states that the maximum permitted building height measures from grade to uppermost point of the roof surface is 4.3 metres. The applicant is requesting permission to permit to allow the building height to be 6.91 metres, a difference of + 2.61 metres, to facilitate the proposed development.

Variance Four: Residential Driveway

Section 5.6.2 vii) of the Town's Zoning By-law states that no residential driveway shall extend into the rear yard except where the residential driveway leads to a detached garage or carport located in the rear yard. The applicant is requesting permission to allow for the existing driveway to remain in the rear yard despite not servicing a detached garage/carport, to facilitate the proposed development.

Consultation

Public Consultation

Notice for the hearing was provided pursuant to the Planning Act on June 8, 2026. As of the writing of this report on June 17, 2026, staff have not received any comments from members of the public.

Agency Consultation

No objections were filed with respect to the variance application from Town staff or external agencies.

Development Services Comments

The applicant has requested a minor variance application to seek relief from four sections of the Town's Zoning By-law to facilitate the conversion of an existing detached garage into an Additional Residential Unit (ARU) which will utilize existing property conditions (residential driveway).

Variance One: Minimum Rear Yard Setback



Consultation

The applicant is requesting permission to allow a minimum rear yard setback of 1.29 metres, a difference of - 0.21 metres, to facilitate the proposed development. The existing detached garage is located 1.29 metres from the rear lot line which currently exceeds the requirement of 0.60 metres for a detached garage in the rear yard. However, once converted into an Additional Residential Unit, the rear yard setback is required to be 1.5 metres to ensure appropriate setbacks between residential units and adjacent properties. Planning staff are of the opinion that the variance required to allow for a deficient rear yard setback for the proposed ARU is appropriate given that the property backs onto a Public School and no tree removal is required. The ARU will be sufficiently setback from the School and Planning Staff do not foresee any negative impacts.

Variance Two & Three: Size of ARU & Building Height

The applicant is requesting permission to allow the gross floor area to be 16.4% of the lot area of 163.5 sq metres, a difference of + 6.4% and + 53.5 sq metres, to facilitate the proposed development. Further, the applicant is requesting permission to permit to allow the building height to be 6.91 metres, a difference of + 2.61 metres, to facilitate the proposed development. As noted above, the existing detached garage is being utilized and converted into an ARU which Planning staff acknowledge requirements deviations from the Town's Zoning By-law.

The existing dwelling and detached garage are currently under the maximum lot coverage requirement and Planning staff do not feel that the size and building height of the existing detached garage will impact the neighbouring properties once converted into an ARU. The intent of the ARU provisions will be applicable.

Variance Four: Residential Driveway

The applicant is requesting permission to allow for the existing driveway to remain in the rear yard despite not servicing a detached garage/carport, to facilitate the proposed development. Planning staff do note that there will be no exterior changes to the existing driveway however, as the Zoning By-law applies, however, the garage itself will not be considered a detached garage but an ARU which requires this variance permission. Planning staff do not have any concerns.

Planning staff have reviewed the requested variances and offer no objection to their approval. Planning staff are of the opinion that the requested variance is minor in nature, conforms to the general intent of both the Official Plan and Zoning By-law and is desirable for the development and use of the subject property.

Respectfully submitted,

Taylor Wellings



The Corporation of the Town of Milton

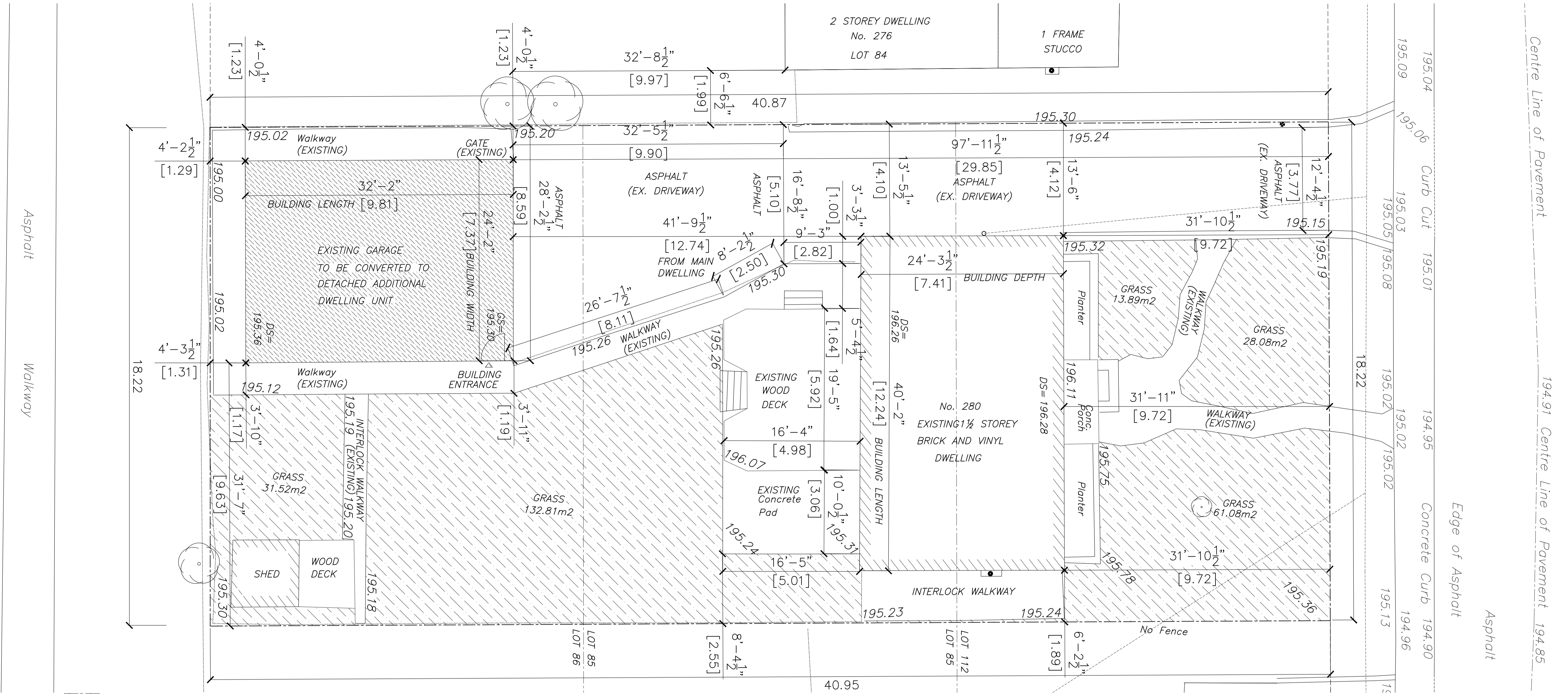
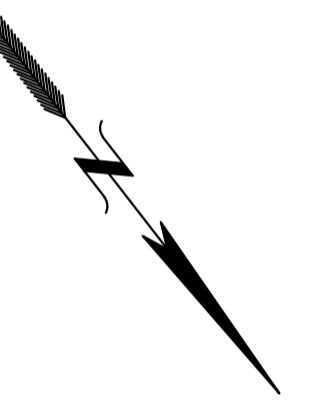
File #:
A-26-026M
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Taylor Wellings, MSc (PI), MCIP, RPP
Planner, Development Review
For questions, please contact:

Phone: Ext. 2311

Attachments
Figure 1 - Site Plan

SIDNEY STREET



SOFT LANDSCAPING

SITE DATA	
LOT No.	PART OF LOTS 85,86,&112 REGISTERED PLAN 96
STREET No.	280 SYDNEY STREET
ZONING	RLD1
LOT AREA	748.1 m2 8053 sq.ft.
LOT FRONTAGE	18.22 m 59'-10"
LOT DEPTH	40.98 m 134'-6"

SITE DATA	PERMITTED		PROPOSED	
	PRINCIPAL DWELLING UNITS	1	1	1
ADDITIONAL DWELLING UNITS ADU/ARU	1	1	1	1
TOTAL BUILDING COVERAGE MAX - 40%	299.2 m2	163.6 m2	137.6 m2	1483 sq.ft.
PROPOSED ADU GFA	163.5 m2	1760 sq.ft.	6.91 m	22'-8"
PROPOSED ADU HEIGHT	6.91 m	22'-8"	7.06 m	23'-2"
PROPOSED BUILDING DEPTH	7.06 m	23'-2"	9.86 m	32'-4"
PROPOSED BUILDING LENGTH	9.86 m	32'-4"		

SITE DATA	REQUIRED		PROPOSED	
	MAIN DWELLING PARKING SPACES	MIN 2	2	2
ADU PARKING SPACES	MIN 1	1	1	1
ADU PARKING SPACE DIMENSION	MIN 2.75m X 5.5m	8.59m X 9.90m	151.61 m2	1631.89 sq.ft.
DRIVEWAY AREA			277.73 m2	2989.44 sq.ft.
LANDSCAPED AREA			302.53 m2	3256.41 sq.ft.
LANDSCAPED AREA %	30%	37%		
HARD SURFACE AREA			1.5 m	29.85 m 97'-11 1/2"
ADU FRONT YARD SETBACK	1.5 m	29.85 m	1.5 m	4'-2 1/2"
ADU REAR YARD SETBACK	1.5 m	1.31 m	1.5 m	4'-1/2"
ADU INTERIOR SIDE YARD SETBACK	1.5 m	1.23 m	1.5 m	9.63 m 31'-7"
ADU INTERIOR SIDE YARD SETBACK	1.5 m	9.63 m		

PLAN OF PART OF LOTS 85, 86, AND 112, REG'D. PLAN 96 - TOWN OF MILTON
 AS TAKEN FROM SURVEY BY ALEX MARTON SURVEYOR
 DATED MARCH 3RD, 2026



SIGNATURE

No.	Description	Date

280 SYDNEY STREET

Site Plan

Date	2026-03-12	A2
Drawn by	AB	
Checked by	NB	Scale 1:200



The Corporation of the Town of Milton

Report To: Committee of Adjustment and Consent

From: Taylor Wellings, MSc (PI), MCIP, RPP

Date: June 25, 2026

File No: A-26-028M

Subject: 10725 Louis St. Laurent

Recommendation: THAT the application for minor variance **BE APPROVED SUBJECT TO THE FOLLOWING CONDITIONS:**

1. That the development shall be constructed in accordance with the site plan prepared by Glenn Piotrowski Architect and dated and stamped by Town Zoning on May 22, 2026.
2. That a Building Permit be obtained within two (2) years from the date of the decision; and
3. That the approval be subject to an expiry of two (2) years from the date of decision if the conditions are not met, if the proposed development does not proceed and/or a building permit is not secured.

General Description of Application

Under Section 45(1) of the Planning Act, the following minor variance to Zoning By-law 016-2014, as amended, has been requested to permit:

1. To allow a maximum floor area of 82.37 sq metres and 170.38 square metres for two detached accessory waste storage structures, a difference of +63.37 sq metres and +150.38 square metres; and
2. To allow a maximum height of 3.05 metres for the two detached accessory waste storage structures, a difference of + 0.50 metres.

The subject property is known as 10725 Louis St. Laurent which forms part of the James Snow Business Park. The subject application applies to one of the buildings located on the Oxford development, identified as Building C, which is located in Phase 1 and was Site Plan approved in April 2023. Building C is 101, 508.6 square metres and will be used for industrial and warehouse distribution.

The subject application is to obtain relief from the maximum floor area permitted for two detached accessory structure for waste storage as well as allow for an increase in height for both



General Description of Application

structures. The application has come forward to assist with the existing industrial operations on site and allow for accessible third party access near the rear of the building.

Official Plan Designation (including any applicable Secondary Plan designations)

The subject property is designated as Business Park Area in the Town of Milton's Official Plan and the Derry Green Corporate Business Park Secondary Plan. In the Town's Official Plan and Secondary Plan, the Business Park Area designation is an employment designation which permits the full range of light industrial and office uses, subject to other policies.

It is Staff's opinion that the proposal is in conformity with the Town of Milton Official Plan.

Zoning

The subject lands are zoned Site Specific Business Park Zone (M1*293) which permits the industrial warehousing/distribution use that is proposed for the subject property.

Variance One: Maximum Floor Area (for an Accessory Waste Storage Structure)

Section 4.14.2 iii) Table 4G of the Town's Zoning By-law states that the maximum floor area permitted for a detached accessory waste storage building or structure is 20 square metres. The applicant is requesting to allow for a maximum floor area of 82.37 sq metres for one detached structure and 150.38 sq metres for another accessory waste storage structure, a difference of + 63.37 sq metres and +150.38 sq metres, to facilitate the proposed development.

Variance Two: Maximum Height (for an Accessory Waste Storage Structure)

Section 4.14.2 iii) Table 4G of the Town's Zoning By-law states that a maximum height of 3.0 metres is permitted for a detached accessory waste storage building and structure in a non-residential zone. The applicant is requesting permission to allow a maximum height of 3.05 metres for two detached waste storage structures, a difference of + 0.50 metres, to facilitate the proposed development.

Consultation

Public Consultation

Notice for the hearing was provided pursuant to the Planning Act on June 8, 2026. As of the writing of this report on June 17, 2026, staff have not received any comments from members of the public.

Agency Consultation

No objections were filed with respect to the variance application from Town staff or external agencies.



Consultation

Development Services Comments

The applicant has requested a minor variance in order to facilitate the proposed development of an industrial/distribution building (Building C) on the subject property. The application has come forward to assist with the existing industrial operations on site and allow for accessible third party access near the rear of the building. The applicant has proposed two accessory structures to accommodate waste storage, and those structures will exceed size and height limitations.

Variance One: Maximum Floor Area (for an Accessory Waste Storage Building)

The applicant is requesting to allow for a maximum floor area of 131 square metres for a detached accessory waste storage structure, a difference of + 111 square metres, to facilitate the proposed development. Planning staff do not have concerns about the requested increase in the maximum floor area for two accessory waste structures as it is appropriate given the size of the existing industrial building and operation. Further, the location near the rear of the building will be shielded from James Snow Parkway.

Variance Two: Maximum Height (for an Accessory Waste Storage Structure)

The applicant is requesting permission to allow a maximum height of 3.05 metres for two detached waste storage structures, a difference of + 0.50 metres, to facilitate the proposed development. Planning staff are of the opinion that the proposed increase in height is minor in nature and appropriate given the industrial operations on site and specific to this building. Planning staff do not foresee the variance request impacting the surrounding industrial sites negatively.

Planning staff are of the opinion that the requested variances all positively contribute to the design and overall efficiency of the site's layout and function of the proposed building. Planning Staff are of the opinion that the requested variance is minor in nature, conforms to the general intent of both the Official Plan and Zoning By-law and is desirable for the development and use of the subject property. As such, Planning Staff have reviewed the requested variance and offer no objection to its approval.

Respectfully submitted,

Taylor Wellings

Taylor Wellings, MSc (PI), MCIP, RPP
Planner, Development Review

For questions, please contact:

Phone: Ext. 2311



The Corporation of the Town of Milton

File #:
A-26-028M
Page 4 of 4

Attachments

ZONING & OTHER GENERAL INFORMATION	
CURRENT USE	VACANT LAND
PROPOSED USE	BUSINESS PARK (M1*xxx3)
BUILDING SETBACKS (TOWN OF MILTON)	FRONT YARD 9.0m - LOUIS ST. LAURENT AVENUE SIDE YARD 3.0m - EAST REAR YARD 12m - CLARK BOULEVARD
OTHER LIMITATIONS (TOWN OF MILTON)	4.5m LANDSCAPE BUFFER REQUIRED ABUTTING STREET LINE

OVERALL SITE STATISTICS	IMPERIAL	METRIC
TOTAL PROJECT SITE AREA	53.9 AC	21.83 Ha (218,322 m.sq)
LOT COVERAGE		46.5%
SOFT LANDSCAPE AREA		11.2% (24,454m.sq)
PARKING, LANES, ACCESS		20.3% (44,323m.sq)
WORK YARD, OUTSIDE LOADING AREA		22.0% (48,036m.sq)

OVERALL BUILDING STATISTICS		
BUILDING - C (EXISTING)	1,092,630 s.f.	101,508.6 m.sq
NEW UNIT	26,458.76 s.f.	475.5 m.sq
MAXIMUM BUILDING HEIGHT		16.343m

OVERALL PARKING STATISTICS			
Industrial Parking Rate: Based on the Town of Milton requirement. Building GFA minus 10%, then 1 space/100 m2 (first 5,000 m2, plus 1 space per 200 m2 thereafter. Standard stall size 2750x5800.	Bldg Area	Stalls required	Stalls provided
BUILDING - C (EXISTING)	101,508.6 m2	482	626
NEW UNIT		25	

DESIGNATED PARKING REQ'TS (FOR THE NEW UNIT)	
Based on number of required parking stalls for 13-100 stalls, 4% accessible parking spaces required. Type A (3400x5800) and Type B (2750x5800)	
TOTAL DESIGNATED BARRIER FREE STALLS REQUIRED	1 STALLS

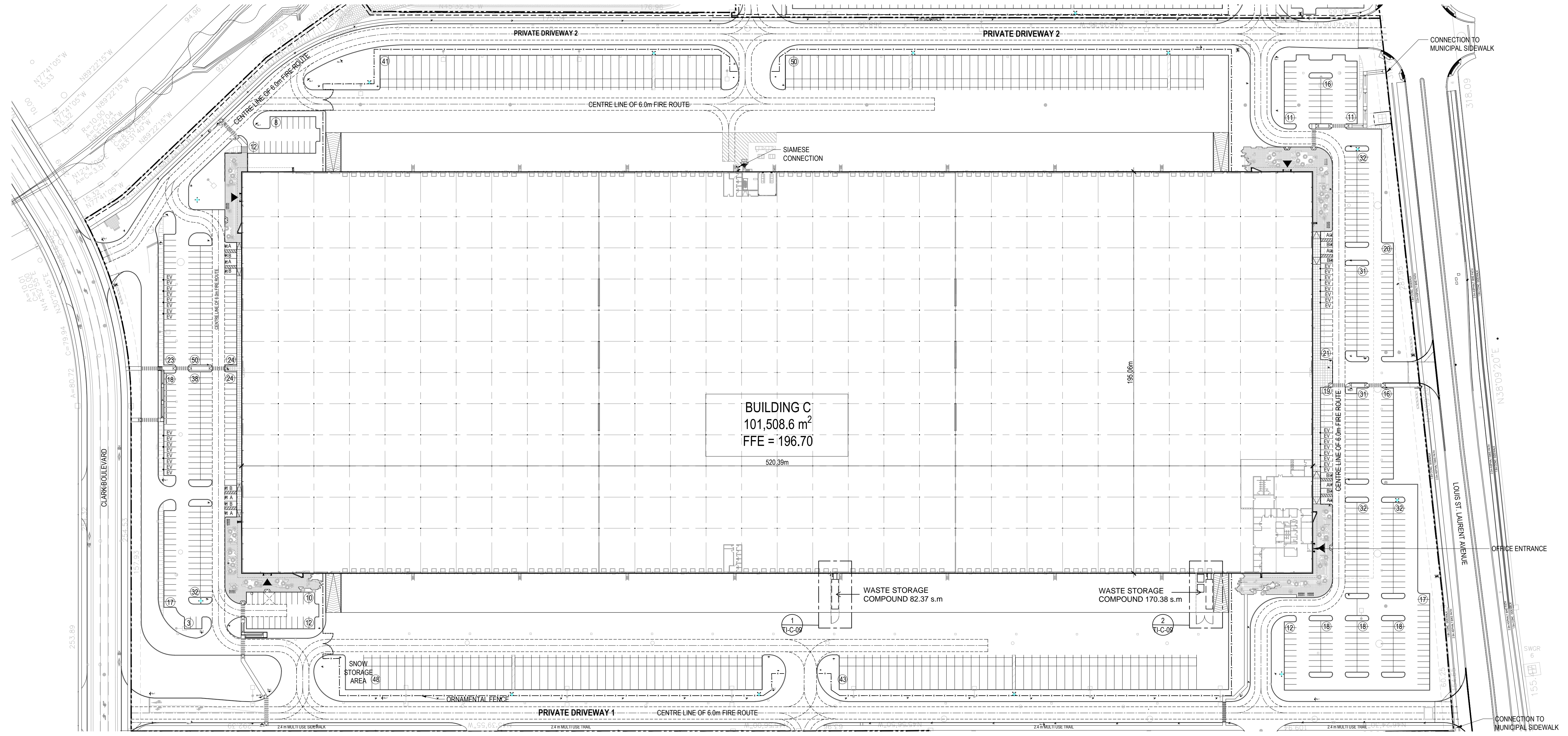
BICYCLE PARKING REQUIREMENTS		
For commercial, employment and institutional uses minimum number of spaces is 3% of required parking spaces, up to a maximum of 5 spaces.		
BUILDING - C (EXISTING)	Stalls required: 5	Stalls provided: 20
EV READY PARKING STALLS	Stalls required: 29	Stalls provided: 32
Provide 6% of parking stalls as 'EV' ready		

LOADING POSITION STATISTICS		
For GFA equal to 7441m.sq and greater, provide 3 plus 1 additional loading space for each 9300m.sq or fraction thereof in excess of 7441m.sq. Loading space size is 3500x12,000 with minimum vertical clearance of 4200mm.		
BUILDING - C (EXISTING)	Stalls required: 14	Stalls provided: 174
TRAILER STAGING POSITIONS PROVIDED		180

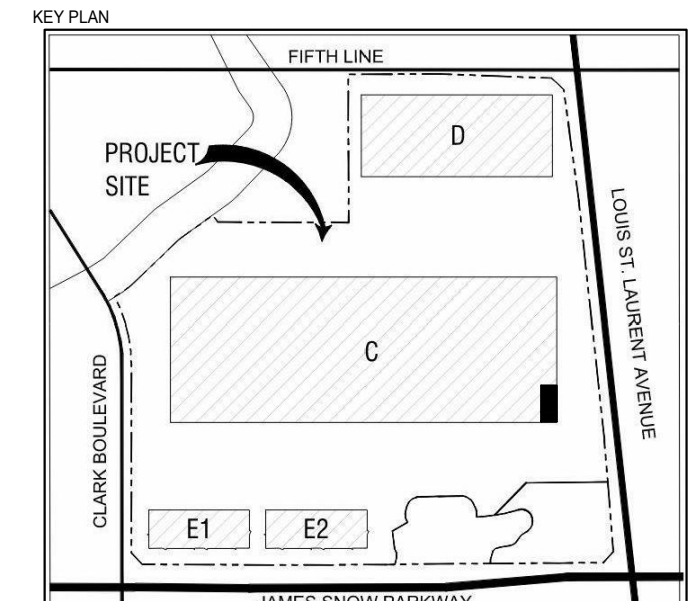
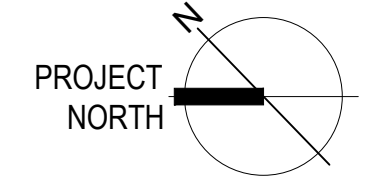
SITE PLAN LEGEND

<ul style="list-style-type: none"> FI: FIRE HYDRANT WITH 4 SAFETY BOLLARD 1.5m FROM CURB EDGE SH: SANITARY MANHOLE SM: STORM MANHOLE WM: DOM. WATER METER & BACK FLOW PREVENTER CB: CATCH BASIN CBMH: CATCH BASIN MANHOLE DCB: DOUBLE CATCH BASIN HC: BARRIER FREE PARKING SIGN PA: PAINTED ACCESSIBILITY BARRIER FREE SYMBOL BE: BUILDING ENTRANCE ACCESS DOOR PE: PRINCIPAL ENTRANCE PL: PROPERTY LINE HA: HEAVY DUTY ASPHALT LA: LIGHT DUTY ASPHALT MW: MONITORING WELL FR: FIRE ROUTE BS: BI-FOLD SWING METAL GATE, POWER OPERATED. NOTE: GATE AND POWER TO BE INSTALLED PROVIDED BY THE TENANT. 	<ul style="list-style-type: none"> LS: SIAMSESE CONNECTION HT: PROPOSED HYDRO TRANSFORMER HP: HYDRO POLE LS: LIGHT STANDARDS WP: WALL PACK TL: TERRAIN LIGHTING GARDEN SPOTLIGHT TYP.(REF. TO ELECT.) LP: LIGHT POLES WITH 3 LIGHT FIXTURES WITH 4M, 3.5M AND 3M AFF. TYP.(REF. TO ELECT.) CR: CURB CUT & RIVERSTONE ROCK SPILLWAY PC: BARRIER FREE CURB RAMP PC: MARKED PEDESTRIAN CROSSING, CONTRAST PAINTED STRIPPING EV: ORNAMENTAL FENCE REFER TO LANDSCAPE EV: ELECTRIC VEHICLE CHARGER CD: BARRIER FREE RAMP CW WITH CURB DEPRESSION IN COMPLIANCE WITH OBC 3.8.3.2 AND REFER TO CITY STANDARD FR: FIRE ROUTE SIGN, MAX. 45m(150') 1.5m FROM THE EDGE OF CURB GR: GALVANIZED METAL GUARDRAIL, REFER TO LANDSCAPE FOR DETAILS TS: TRAFFIC SIGNAGES REFER TO TRAFFIC DRAWING FOR DETAILS
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REV	DATE	DESCRIPTION
01	APR 28, 2026	ISSUED FOR COMM OF ADJUSTMENT



SURVEY INFORMATION BASED ON SURVEY '2102742d-mod' PROVIDED BY R-PE Surveying Ltd. received 2022-03-07 (UNSIGNED)



Glenn Piotrowski Architect

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OAKVILLE, ON.
L6J 2Z6
905.336.8855

CLIENT
ID LOGISTICS

PROJECT
**TENANT IMPROVEMENTS
BUILDING C ID LOGISTICS**

10725 LOUIS ST LAURENT AVE
MILTON, ONTARIO, CANADA

SITE PLAN - FOR WASTE STORAGE ENCLOSURES

DRAWN BY	BKY
DATE	2026-04-17
SCALE	AS SHOWN
PROJECT NO.	20xx-xx
SHEET NO.	

TOWN OF MILTON
DEVELOPMENT SERVICES
M1*293 ZONE
ZONING - REVIEWED FOR C of A
robin campbell MAY 22, 2026
ZONING OFFICER SITE

TI-C-01a