



The Corporation of the Town of Milton

Report To: Council

From: Jill Hogan, Commissioner, Development Services

Date: December 8, 2025

Report No: DS-070-25

Subject: Trafalgar Tertiary Plan

Recommendation: THAT Council endorse the Trafalgar Tertiary Plan attached as Appendix 1;

AND THAT the Commissioner of Development Services have the authority to approve any updates required to the Road Network Assessment;

AND FURTHER THAT prior to any draft plan approval within the Trafalgar Secondary Plan, the Master Environmental Servicing Plan shall be finalized and endorsed by the Conservation Halton Board.

EXECUTIVE SUMMARY

- Council approved the Trafalgar Secondary Plan in March 2019, and the Secondary Plan was finalized through the Ontario Land Tribunal in July 2024.
- The Trafalgar Secondary Plan requires a landowner led Tertiary Plan to be completed and endorsed by Council prior to the approval of development applications.
- The Trafalgar Landowner's Group submitted the Tertiary Plan for review in fall of 2024, and Town Staff and agencies have reviewed and commented on the submission.
- Staff is now recommending Council endorse the final version of the Trafalgar Tertiary Plan, attached as Appendix 1, for development applications to proceed within the Trafalgar Secondary Plan Area.

REPORT

Background
Secondary Plan

Background

The Trafalgar Secondary Plan, as approved by the Ontario Land Tribunal, sets out a process for a Tertiary Plan to be submitted to the Town as the first step to realizing development within the Trafalgar Secondary Plan Area.

The Trafalgar Secondary Plan is located in Milton's Sustainable Halton Urban Expansion Area and is generally bounded by Derry Road to the north, Eighth Line to the east and the Greenbelt Plan Area to the south and west. The Trafalgar Secondary Plan is envisioned to be developed as a mixed-use, transit- supportive, and higher density community.

The Trafalgar Secondary Plan was adopted by Council on March 25, 2019. Following Council adoption, the Secondary Plan was submitted to Halton Region for approval. Halton Region issued its approval of the Secondary Plan with modifications in September 2022. The Secondary Plan was then appealed. In July 2024 the Ontario Land Tribunal issued its decision to approve the Secondary Plan with revisions, which then came into effect.

Tertiary Plan

A Tertiary Plan is intended to implement a Secondary Plan and provide more detail than the Secondary Plan to inform development. It must be completed by landowners to the satisfaction of the Town, in consultation with agencies. The Tertiary Plan must demonstrate how development can proceed in a coordinated manner, address infrastructure servicing, natural hazard management and risk mitigation, natural heritage protection, road networks, parks, schools, places of worship and other community infrastructure. It ultimately provides a framework for coordinating neighbourhood subdivision development that spans multiple owners and properties to ensure that the policies and intent of the Secondary Plan are achieved.

The Trafalgar Secondary Plan is drafted to require the submission of a Tertiary Plan by the Trafalgar Landowner's Group prior to proceeding to development applications. Further, the Trafalgar Secondary Plan contains policy that outlines Tertiary Plan submission requirements and required supporting studies. The Secondary Plan also contains policies that allow for increases in population and employment, provided that appropriate background studies that address the implications of the additional population on community uses and infrastructure are completed and accepted by the Town.

While the Secondary Plan was prepared to accommodate 32,000 residents and 4,000 jobs, the Trafalgar Tertiary Plan attached as Appendix 1 provides for population and employment totalling 45,000 people and 6,040 jobs. In the Town's Residential and Non-Residential Needs Analysis Study, these population and employment increases were accounted for and have

Background

been incorporated into both the Regional Best Planning Estimates and the Town's updated population and employment targets and forecasting within the Town's new Official Plan.

Discussion

Tertiary Plan Process

The Tertiary Plan has been reviewed through an ongoing collaborative process between Town Staff, the landowner's group and various agencies. Staff worked with the Trafalgar Landowner's Group through 2024, as the Trafalgar Secondary Plan was being finalized through the Ontario Land Tribunal, to establish the process and precise submission requirements for the Tertiary Plan and reviewed initial concepts. Staff received the first formal submission of the Trafalgar Tertiary Plan in October 2024. This submission was subsequently circulated to agencies for review, followed by a Tertiary Plan agency meeting in November 2024. Comments were provided to the Landowner's Group at the end of November. Throughout November, meetings were held with Town departments and external agencies, as needed, to discuss questions and comments. Topic specific meetings continued through Q1 of 2025 on items, such as schools, parks and Financial Impacts Assessment inputs.

The Town received the second submission of the Tertiary Plan in March 2025 and again circulated to Town departments and external public agencies for comment and review. Comments were provided through April and May 2025.

Staff received the final Trafalgar Tertiary Plan, that satisfied all comments received to date. Staff are recommending that Council endorse the Trafalgar Tertiary Plan, attached as Appendix 1, to be used to assist in the review of development applications within the Trafalgar Secondary Plan.

Background Studies and Supporting Documents

As mentioned above, a series of updates were completed by the landowner's group to support the Tertiary Plan, including a Community Facility and Human Services Impact Report Update, a Parkland, Recreation and Library Report Update, a Population, Employment and Housing Analysis Update, and an Area Servicing Plan Update, all of which have been reviewed and deemed to be complete and satisfactory by the Town and applicable agencies. As a part of these studies supporting the population increase and general make up of the Trafalgar Tertiary Plan, it was determined that to accommodate the 45,000 people a total of 17,260 units is estimated to be required, 9,403 of which would contribute to achieving the

Discussion

30% target of affordable housing, assisted housing, stacked townhouses, back-to-back townhouses or apartments established in the Secondary Plan. Further an additional two places of worship may be required to meet the needs of the community. A total of eight elementary schools and one secondary school is planned to be provided in the Trafalgar Secondary Plan Area.

The original background studies for the Trafalgar Secondary Plan area were completed in 2019 for the Trafalgar Corridor as a whole, which includes the Agerton Secondary Plan area. Together the two Secondary Plan areas require a community centre and fire station which will be accommodated in the Agerton Secondary Plan area.

A Road Network Assessment has also been completed and reviewed by Town and Regional Staff. Outstanding comments have been addressed in a recent version, which is anticipated to be accepted as the final version following review and confirmation from Town and Regional Transportation Staff.

A Master Environmental Servicing Plan has been underway over the last few years, being prepared in support of the Tertiary Plan. The Town's consulting team has been working collaboratively with the landowner's group and Conservation Halton to finalize this report, including, among other items, the management strategy related to natural hazards. It is a requirement of the Secondary Plan that the Master Environmental Servicing Plan be completed and accepted by the Town in consultation with Conservation Halton. In this report Staff are recommending that Council endorse the Tertiary Plan but, prior to any draft plan approval within the Trafalgar Secondary Plan, the Master Environmental Servicing Plan must be finalized and endorsed by the Conservation Halton Board. This endorsement is anticipated for Q1/Q2 2026.

The Town has also entered into a Master Parkland and Trails Agreement with the Trafalgar Landowner's Group which determined the final parkland calculations, as consistent with the Trafalgar Secondary Plan and Tertiary Plan background studies.

Finally, as a part of the Tertiary Plan process the Town completed a Financial Impact Study, which required input from the Tertiary Plan to complete. The results of the Financial Impact Study are addressed below in detail.

Implementation

As discussed, the Trafalgar Tertiary Plan, attached as Appendix 1, was developed by the Trafalgar Landowner's Group, in consultation with various Town departments and external

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agencies. The development of a detailed Tertiary Plan is intended to assist in streamlining the review of development applications, provided the application is consistent with the Tertiary Plan which has already been subject to detailed scrutiny and review.

A development application that is not entirely aligned with the Tertiary Plan may still be considered and approved by the Town provided it is in conformity with the Trafalgar Secondary Plan. The Trafalgar Secondary Plan is a chapter of the Town's Official Plan and is the ultimate conformity test for development applications. The Tertiary Plan is a guide to how development will be coordinated, and to ensure appropriate staging of development and complete communities, demonstrating how development may proceed. Deviations from this plan will still be considered but may require further review by Town Staff and external agencies if major shifts in community uses, or infrastructure are being proposed through a development application.

The build out of the Trafalgar Secondary Plan, particularly Phase 2, is anticipated to take several years, as such, there should be consideration in support of deviations from the endorsed Tertiary Plan at the time of the development application, when supported by an appropriate planning justification report, or other supporting documents as determined by Staff through the application process.

Financial Impact Study

Through the Official Plan and other Council-approved policies, the Town ensures that growth occurs in a fiscally responsible manner. This requires the completions of a comprehensive Fiscal Impact Study (FIS) that considers projected growth, service levels and changes within the community, and their resulting implications on the Town's cash flows, funding sources and overall financial health.

The most recent comprehensive FIS was presented to Council through report CORS-056-21. That study assessed growth in its entirety for the Town for the period 2021 to 2041, inclusive of the development that was anticipated for the Trafalgar Secondary Plan Area (Trafalgar). As was noted in CORS-056-21, the findings of the comprehensive study were expected to be further assessed through the secondary and tertiary plan processes.

Staff have undertaken that further assessment of the fiscal impacts of Trafalgar by leveraging the framework that was utilized in the 2021 FIS. The update evaluated Trafalgar-specific data, with a combination of updated inputs from the Secondary/Tertiary process (for example, revised growth estimates, phasing and infrastructure assumptions, updated property tax rates, etc.) as well as certain unit rate assumptions that were established

Discussion

through the 2021 FIS and inflated to current day values. Where costs or revenues are associated with several geographic areas within the Town, only a pro-rated portion that could be attributed to Trafalgar was reflected in the analysis.

Summary tables for the Trafalgar Specific FIS update are presented as Appendices 2-5 of this staff report. They include:

- Growth projections including population, employment and assessment growth, including phasing
- Infrastructure projections (both constructed and assumed) in relation to Trafalgar
- Cash flow of the growth-related capital investments required in relation to the potential growth-related revenue sources (i.e. development charges) that will be available to the Town
- Operating budget implications that are forecasted in relation to Trafalgar

The analysis was undertaken in 2025 in order for the impacts to be evaluated against the Town's existing financial position. From this analysis, the following observations can be made:

- the Trafalgar area is expected to add 45,180 people in 17,260 residential units, along with 6,040 jobs in 128,050 square metres of employment space during the planning horizon out to 2051;
- the Town's assessment base is expected to grow by \$6.9 billion (based on CVA values as of January 1, 2016), of which 98% will relate to residential growth. The total assessment growth represents a 24% increase in the Town's assessment base;
- the Town's asset base is expected to grow by \$709 million in relation to infrastructure that is attributable to the growth in Trafalgar (including both assets that are constructed by the Town or assumed from developers). This represents a 20% increase to the Town's existing asset base as reported in the 2025 Asset Management Plan;
- an evaluation of the anticipated development charge revenues (on the basis of the existing DC rates) relative to Trafalgar's share of the Town's future growth-related capital program shows that:
 - o an initial deficit of up to \$40 million is anticipated in relation to Trafalgar's share of early investments that may be required within the Town's capital forecast;
 - o an aggregate reserve fund balance of approximately \$91 million may subsequently accumulate until such time as the Community Centre for the Trafalgar proceeds to construction;
 - o an ending deficit of approximately \$39 million is presented at the end of the planning horizon. This shortfall is a result of the use of the Town's existing DC

Discussion

- rates in the analysis along with updated cost estimates, with respect to the community centre, transit and fire. As the DC rate will have the opportunity to be updated through future by-law updates, the final reserve fund balance will have the opportunity to be adjusted through those exercises.
- based on existing property tax rates and Town financial policies, as well as the extension of existing service levels, the Trafalgar area is projected to create a \$6.7 million budget pressure that would be realized over the 25+ year planning horizon. This pressure equates to an approximate 6.0% increase to the approved 2025 property tax levy. In regard to this forecasted pressure, it is important to note:
 - this pressure on the property tax rate directly relates to the Town's existing infrastructure deficit, as the current property tax rates are not sufficient to sustainably fund the existing service levels. The projections undertaken for Trafalgar have assumed that, in accordance with existing Town policy, the new infrastructure that is constructed or assumed for Trafalgar will be fully funded in the operating budget in order to not increase the Town's funding deficit. The result is the related pressure on the existing property tax base.
 - If the targets established in the Town's approved multi-year budget strategy (as approved through ES-016-24) are achieved, the tax rate pressure from growth areas such as Trafalgar would be lessened (if not fully eliminated), as the revenues generated from the area would provide for the service levels that are delivered.
 - Since the analysis assumed existing service levels, impacts related to potential service level increases (such as electrification of transit vehicles, increases to transit service hours per capita, etc.) are not reflected in the findings presented, and would form additional cost pressures when introduced.

It is important to note that in addition to potential future service level changes, the analysis would also be impacted by any future changes in legislative or any additional requirements that the Town may be subject to (such as future potential contributions to Provincial infrastructure or services).

The findings for the Trafalgar specific analysis are largely consistent with the broader findings from the Town's most recent comprehensive FIS that was undertaken in 2021. The updated assumptions for Trafalgar that have been acquired through the secondary and tertiary planning processes will also be reflected in the Town's next comprehensive FIS process, which is currently anticipated in 2026/2027.



Financial Impact

The amount and pace of growth can have a notable financial impact to the Town. The overall financial pressure is further compounded by the annual incremental investments required to address current shortfalls in infrastructure funding and master plan implementations. Therefore, the preparation of fiscal impact studies is essential to support Council in the management of growth and the goal of financial sustainability. Fiscal impact studies provide the long-term outlook that can help inform annual decisions through the budget process, as well as the strategic decisions required through master plan exercises and secondary planning processes. The projections presented are contingent on the continuation of the Town's existing financial policies and strategies with respect to capital financing and have been based on the Town's existing levels of service.

As described in further detail above, the FIS that was undertaken specifically for Trafalgar resulted in findings that were consistent with the 2021 FIS that evaluated growth for the entirety of Milton. Specifically, the Trafalgar FIS identified continued pressure on the existing property tax base so long as the local portion of the tax rate does not provide for a sustainable level of infrastructure funding for the service levels that are currently provided to the community. Should the Town continue to implement its existing strategies with respect to annual infrastructure funding, the pressure on the existing tax base that results from continued growth is expected to lessen over time.

With respect to cash flow, and specifically the collection of development charges (DCs) relative to the related capital expenditures, the current estimates suggest that following an initial shortfall, Trafalgar will contribute towards growth in the Town's DC reserve funds until such time as the new community centre will be constructed, after which a shortfall can be expected once again. It should be noted that these estimates are premised on the existing DC rates, and should those rates be adjusted through the 2026 (or future) by-law updates, the ending shortfall position may be avoided by the Town. Without a revision to the related DC rates, the Town may be required to re-assess future investment plans in infrastructure or consider alternate funding sources.

Respectfully submitted,

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Commissioner, Development Services

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Attachments

Appendix 1 – Trafalgar Tertiary Plan

Appendix 2 - Trafalgar SPA Growth Projections

Appendix 3 - Trafalgar SPA Infrastructure Value

Appendix 4 - Trafalgar SPA Projected DC Cash Flow

Appendix 5 - Trafalgar SPA Estimated Operating Budget

Approved by CAO
Andrew M. Siltala
Chief Administrative Officer

Recognition of Traditional Lands

The Town of Milton resides on the Treaty Lands and Territory of the Mississaugas of the Credit First Nation. We also recognize the traditional territory of the Huron-Wendat and Haudenosaunee people. The Town of Milton shares this land and the responsibility for the water, food and resources. We stand as allies with the First Nations as stewards of these lands.

PREPARED FOR:
Town of Milton on behalf of the Milton P4 Trafalgar Landowners Group Inc.

NOVEMBER

2025

TRAFALGAR TERTIARY PLAN

Town of Milton



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1 Introduction

“Cities have the capability of providing something for everybody, only because, and only when, they are created by everybody.”

-Jane Jacobs

SGL Planning and Design Inc. has been retained by Milton P4 Trafalgar Landowners Group Inc. (“Trafalgar Landowners”) to prepare a Tertiary Plan for all lands within the Trafalgar Secondary Plan area. The subject lands are organized linearly along Trafalgar Road, from the Greenbelt (approximately mid-block between East Lower Base Line and Britannia Road) to the south and west, Derry Road to the north, and Eighth Line to the east (**Figure 1**).

Envisioned as a complete community, the Trafalgar Secondary Plan is focused on delivering connected neighbourhoods, parks and open space and a mix of uses and built form with densities that support future higher order transit.

In order to realize the vision, the Secondary Plan requires a more detailed Tertiary Plan, the last layer between the policies and implementation by the draft plans of subdivision. The preparation of this Tertiary Plan has been guided by the objectives and detailed policies of the Secondary Plan.



Figure 1. Trafalgar Secondary Plan and Tertiary Plan Study Area boundary

Included in **Appendix A** is a Demonstration Plan that provides one way in which development applications could proceed.

1.1 Document Purpose

The Tertiary Plan supports the approved Secondary Plan. The Secondary Plan establishes a Tertiary Plan Area and identifies the requirements for the preparation of a more detailed plan to be substantially advanced prior to submission of development applications. The Tertiary Plan provides a framework for achieving coordinated development contributing to the Secondary Plan area as a complete community.

The Trafalgar Tertiary Plan provides design direction for the development of this community and reflects the commitment of the Town and development community to good community design and provides a consistent vision to be applied to all development applications.

1.2 Tertiary Plan Format

The Tertiary Plan is organized to highlight the vision, describe the community design, outline how it addresses Secondary Plan policies, urban design guidelines, and detail the phasing and staging of development. The following describes the sections of the Tertiary Plan.



Section 1 provides a general introduction to the Tertiary Plan area, the purpose of the document, summary of supporting studies and describes how the document should be used.



Section 2 outlines the Tertiary Plan vision, describing elements of the concept plan and details of the community structure.



Section 3 outlines how the community design responds to policy and design guidelines for: the Natural Heritage System, parks, open spaces, active transportation, vehicular transportation network, and Local and Neighbourhood Centres.



Section 4 outlines how this Tertiary Plan will help to guide subsequent development applications and implement the long term vision.

1.3 Tertiary Plan Audience

The Tertiary Plan will guide the implementation of the Secondary Plan and has been prepared to assist several different users:



The Development Community:

As the primary user of the Tertiary Plan, the development community will utilize the Tertiary Plan in the design and development of draft plan of subdivision and site plan applications. These development applications will demonstrate how the proposals respond to and meet the intent of the Tertiary Plan.



The Public:

The Tertiary Plan provides the public with confidence that the Town's commitment to the vision of the area will be implemented.



Town Council:

Town Council will endorse the Tertiary Plan ensuring it confirms the vision for the area.



City Staff:

City staff will use the Tertiary Plan to confirm implementation of the Secondary Plan in the review and approval of development applications.

1.4 Background

As part of the approval of Regional Official Plan Amendment (ROPA) #38, additional lands were identified in the Town of Milton to accommodate population and employment growth from 2021 through to 2031. **Figure 2** identifies the lands for growth to serve as Milton’s next urban expansion area and major Secondary Plan areas.

Referred to as “Sustainable Halton Lands” (SHL) or “Milton Phase 4”, these lands are located in the southern and eastern portion of the Town of Milton and include approximately 2,000 hectares (5,000 acres).

Three Secondary Plan areas have been identified for the SHLs, including the ‘Agerton Secondary Plan’, the ‘Trafalgar Secondary Plan’, and the ‘Britannia Secondary Plan’.

These Secondary Plan areas are to be developed as mixed use, transit supportive communities. Through ROPA 38, a Major Transit Station was identified north of Derry Road within the Agerton Secondary Plan area.

1.4.1 Site Context

The Secondary Plan area is approximately 680 hectares in size.

The subject lands are predominantly agricultural, and also contain plant nurseries, golf courses, and residential homes on large lots. Surrounding uses include open spaces, Greenbelt, future strategic employment areas, farmland and agricultural uses. Highway 407 and the City of Mississauga are located further east of the Secondary Plan area.

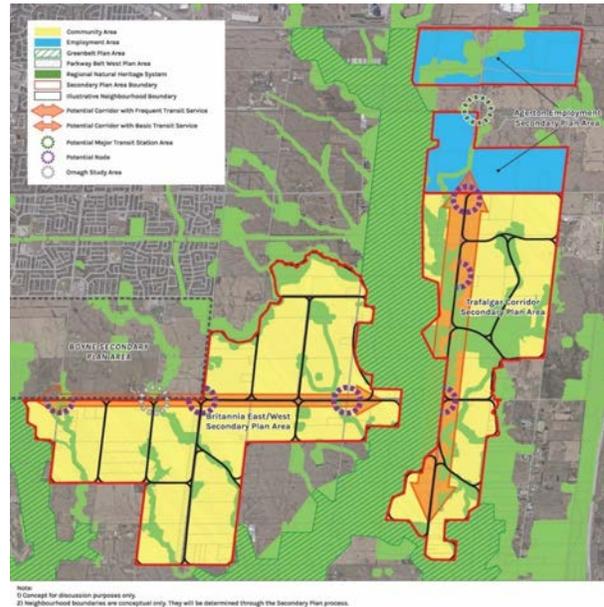


Figure 2. Town of Milton Urban Expansion Area / Sustainable Halton Lands from ROPA 38

The closest community amenities, such as libraries, retail stores and community centres, are located in the Milton Urban Area to the west, and in the City of Mississauga to the east. One public park (Drumquin Park) is located on the north side of Britannia Road, west of Trafalgar Road, not far outside the Secondary Plan area, providing baseball diamonds, track cycling facility and fields for model airplane flying. The subject lands also contain or are in close proximity to several golf courses. There is an existing place of worship in the Secondary Plan area. **Figure 3** provides a map locating community amenities within and surrounding the site. The letter update to the Community Facilities Report, prepared by SGL, identifies the extent of community facilities in the Town of Milton.

The future Secondary Plans of Britannia to the west and Agerton to the north are currently in various stages of planning. Both these areas will contain future amenities and community facilities that may be within walking distance to some residents within the Trafalgar corridor.

1.5 Environmental Conditions

The site contains a number of natural features. The Greenbelt is located along the west limit of the Secondary Plan. The East Branch of Sixteen Mile Creek along with several of its tributaries traverse within the subject lands. The East Branch is one of the many tributaries in the Sixteen Mile

Creek Watershed, and drains southerly to Lake Ontario (**Figure 4**).

The site's existing topography is relatively flat with some localized undulation. The lands generally drain in a southerly direction, with the exception of the southeast quadrant of the lands which generally drain northwesterly to the existing Lower Middle Tributary.

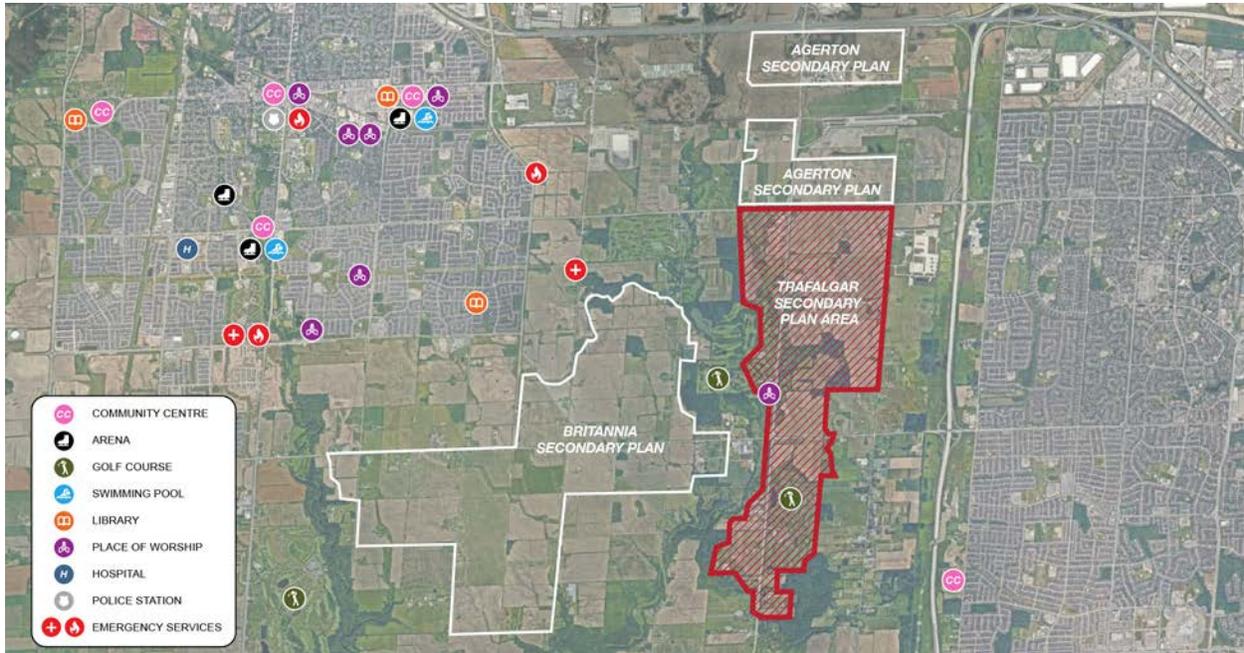


Figure 3. Community amenities within Milton

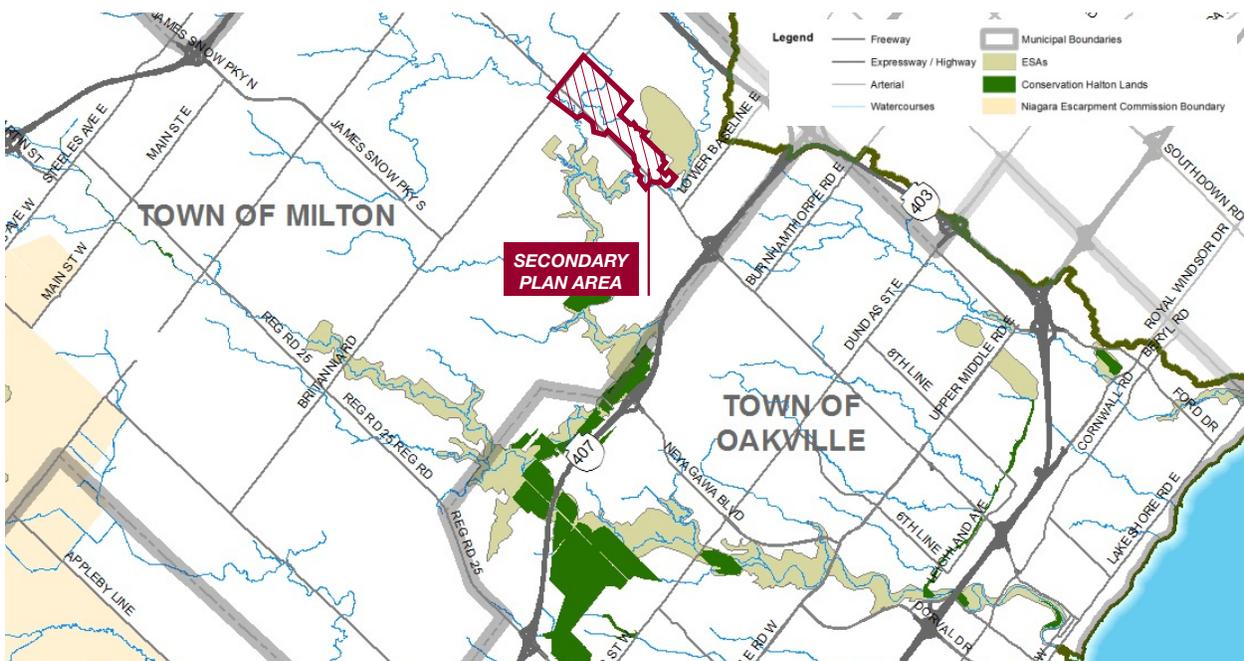


Figure 4. Sixteen Mile Creek Stream Corridors (Conservation Halton, 2015)

1.6 Supporting Studies

1.6.1 Subwatershed Study

The Town of Milton completed a Subwatershed Study (SWS) for the Urban Expansion Area in March 2023. The SWS consisted of four phases including Phase 1 (Background Review and Subwatershed Characterization), Phase 2 (Impact Assessment), Phase 3 (Management Strategies) and Phase 4 (Implementation and Monitoring Plan). The SWS identified constraints and opportunities for development, provided recommendations for protection and management of natural hazard, natural heritage and water resources and provided recommendations for the NHS in the Secondary Plan through refinement of the Regional NHS.

1.6.2 Master Environmental Servicing Plan

The Master Environmental Servicing Plan (MESP) is being undertaken by the Town of Milton and will build upon and refine the recommendations of the South Milton Urban Expansion Area Subwatershed Study (SWS) with respect to servicing and NHS identification and protection. The MESP is an integrated environmental and engineering study supporting the Tertiary Plan on matters such as refinements to the NHS, Stormwater Management/Low Impact Development measures, site grading and servicing, assessments of potential impacts to the NHS, natural hazard management and risk mitigation approaches. In support of the Tertiary Plan, the MESP is substantially advanced for 4 subdivisions in providing assessments of potential impacts to the NHS, identification of design and mitigative measures to demonstrate no negative impacts to that portion of the NHS, approaches to monitoring and identification of future study needs.

1.6.3 Agerton-Trafalgar Transportation Master Plan

The approved 2025 Milton Transportation Master Plan (TMP 2025) is a document which outlines the long-term vision for the future of the Town of Milton's transportation system. More specifically, the TMP 2025 refers to itself as a "guiding document" for a "more balanced, multi-modal future." The TMP 2025 provides policy context for the approach to achieving the vision, which includes an outline of capital improvements and additional studies, as required. Input from staff and decision makers, stakeholders, and residents were used to inform the direction of the TMP 2025.

The TMP 2025 as well as the approved WSP Transportation Master Plan includes a set of objectives that were used to inform the Tertiary Plan and Road Network Assessment (RNA) in support of the Trafalgar Secondary Plan.

1.6.4 Road Network Assessment

The RNA (November 2025) provides an appropriate public street network shown in the Trafalgar Tertiary Plan. The RNA includes the conceptual location, configuration, width and alignment of public roads and high-level traffic modelling to provide arterial and collector road intersection improvements commensurate with phasing / staging of development. Based on the preferred road network in the Town's TMP 2025 for the Trafalgar Secondary Plan and a multi-modal approach, the public street network will optimize opportunities for an integrated transportation network for pedestrians, cyclists, transit, automobiles and goods movement.

1.6.5 Community Facility and Human Services Impact Analysis Report

As part of the original background work for the Trafalgar and Agerton Secondary Plan a Community Facility and Human Services Impact Analysis Report was prepared by the Town’s consultant, Malone Given Parsons Ltd (MGP), dated March 2019. As part of the Tertiary Plan work, an update to that report is required. A letter amending the MGP report was prepared by SGL to update certain sections and tables of that report to reflect the updated population estimates for the Trafalgar Secondary Plan area. The original report was based on lands referred to as the Trafalgar Corridor and included both the Trafalgar Secondary Plan area and Agerton Secondary Plan area. This letter focuses on the Trafalgar Secondary Plan area only and is based on a population of 45,000 for the Trafalgar Secondary Plan.

1.6.6 Population, Employment and Housing Analysis Report

As part of the original Secondary Plan, a Population, Employment and Housing Analysis Report was prepared by MGP, dated March 2019. Based on the proposed population of 45,000 for the Trafalgar Secondary Plan, SGL has prepared an updated letter that revises certain sections and tables of that report to reflect the updated population and employment estimates for the Trafalgar Secondary Plan area.

1.6.7 Parkland, Recreation & Library Gap Analysis Report

In addition, the Parkland, Recreation & Library Gap Analysis for the Trafalgar Corridor – Preliminary Analysis by the Town’s consultant, Monteith Brown Planning Consultants was prepared March 12, 2019. Due to an increase in

the population from 32,000 for the entire Trafalgar Corridor to 2031 to 45,000 for the Trafalgar Secondary Plan area to 2051 an update to this report is also needed. SGL prepared a letter with updates to certain tables and sections to the report to reflect the increase in population of 45,000 for the Trafalgar Secondary Plan which takes into consideration a population of 14,000 for the Agerton Secondary Plan to the year 2051 for the purpose of a multi-use community centre.

1.6.8 Commercial Needs Study Update

In 2018, Ward Economics Inc. prepared a Retail Market Assessment (Ward Study). As part of the Tertiary Plan work, an update to this study is needed. urbanMetrics has prepared a Commercial Needs Study update. The Ward Study focused on both Agerton and Trafalgar Secondary Plan areas and a combined population of 32,000. Since the Ward Study was prepared, the Tertiary Plan includes a higher population for the Trafalgar Secondary Plan, significant changes have happened for retail shopping patterns and the Agerton Secondary Plan area includes a future MTSA. All of which have an impact on the commercial needs for the Trafalgar Secondary Plan. The updated analysis identifies between 580,000 to 840,000 sq.ft. to serve the Trafalgar Secondary Plan population of 45,000.

1.6.9 Area Servicing Plan (ASP) & Area Servicing Plan Addendum (ASP Addendum)

The Town of Milton completed an Area Servicing Plan (ASP) in April 2022 for the Agerton & Trafalgar Secondary Plans through their consultant Wood Environment & Infrastructure Solutions. This study outlined the water and wastewater servicing needs in accordance with Halton

Region's Area Servicing Plan process. An Area Servicing Plan Addendum (ASP Addendum) has been prepared by David Schaeffer Engineering Ltd in support of the Tertiary Plan. The ASP Addendum affirms the water and wastewater servicing needs resulting from the additional population in the Trafalgar Secondary Plan area and supports modifications including optional routing to the internal trunk alignments and connections to the existing water and wastewater infrastructure.

1.7 Policy Framework, Design Guidelines and Standards

The Tertiary Plan conforms to the Council endorsed 2025 Town of Milton Official Plan and the approved Secondary Plan policies, meets the intent of Town design guidelines and reflects existing and agreed to design standards.

The Council endorsed 2025 Town of Milton Official Plan (OP) provides a vision for the development of the Town of Milton to be an engaging, balanced and a connected complete community with a thriving natural environment.

The Tertiary Plan should be read in conjunction with the Trafalgar Secondary Plan, Design Standards, Trafalgar Secondary Plan Urban Design Guidelines (2020), Town of Milton Tall Building Guidelines (2018), and Town of Milton Mid-Rise Guidelines (2018).

2 Tertiary Plan Vision

“That land is a community is the basic concept of ecology, but that land is to be loved and respected is an extension of ethics.”

-Aldo Leopold

2.1 Vision for Trafalgar Secondary Plan Area

The Tertiary Plan presents a detailed vision and guiding principles that direct the implementation of the Trafalgar Secondary Plan area. The vision for the Trafalgar Community was decided through a planning process which included public consultation and a Secondary Plan study by the Town of Milton.

The more detailed vision for the Tertiary Plan was developed through a series of workshops with Town staff. The workshops focused on specific topics on the structure and design elements of the Plan including the NHS, Transportation, Community Facilities, Neighbourhood and Local Centres and Stormwater Management. The vision and community character is expressed through the Tertiary Plan and brings to life the character elements that will create a sense of place and make this area sought-after to live, work and visit. Strong central spines illustrated in **Figure 5** help create a link of compatible, community land uses connected both north/south and east/west by multi-use paths (MUP), trails, parks, schools, and stormwater management facilities all adjacent to a mix of built form and land uses.

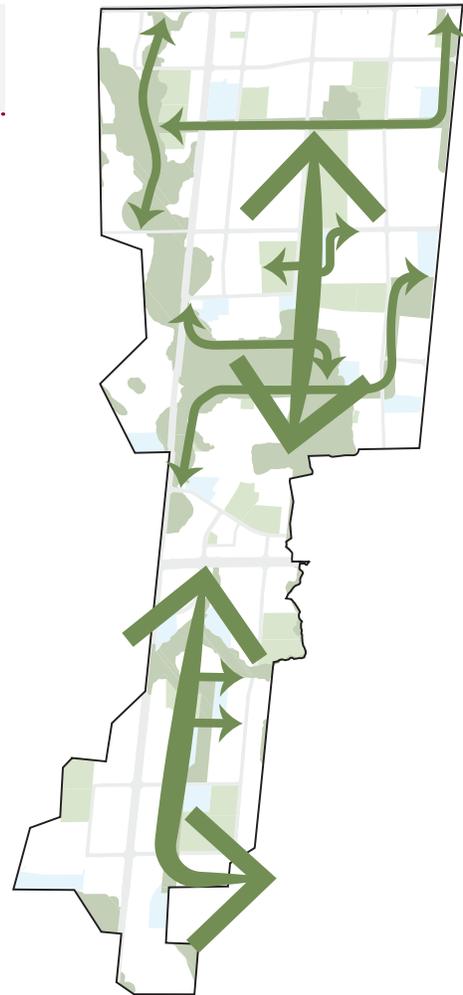


Figure 5. Strong central spines connect the community

The Tertiary Plan helps to establish a new community that will be innovative and resilient. The efficient land use establishes a community that is walkable, provides a mix of uses, and supports future higher order transit.

2.2 Guiding Principles

The Tertiary Plan includes the following guiding principles based on the objectives of the Secondary Plan:



Design and Build Compact Complete Communities



Protect and Embrace the Natural Heritage Systems



Provide a complete and efficient Transportation Network



Create High-quality spaces and public realm

2.3 Community Structure

The community structure described below is intended to inform the development of draft plans and detailed site plans. The following sections describe the structural elements identified in the Secondary Plan and are further refined as part of this Tertiary Plan.

2.3.1 Natural Heritage System (NHS)

The NHS helps inform both the central spine of the community and how connections can be provided throughout. In accordance with Section B.4.9.3.12 of the Town of Milton OP, refinements to the NHS including additions, deletions and/ or boundary adjustments, may occur through an approval process under the Planning Act supported by either a MESP, a Development Area Environmental Functional Servicing Study (DAEFSS), an Environmental Impact Assessment (EIA) or equivalent study accepted by the Town in consultation with the Conservation Authority.

2.3.2 Transportation

The transportation network, which consists of arterial, collector, local roads and trails, provides the detailed organizational layer to the structure of the Secondary Plan. The network contributes to the community's sense of place and plays a large part in establishing pedestrian oriented public realms and an active transportation network.

The final location, configuration, width, and alignment of roads and street hierarchy will be confirmed through detailed draft plans, as part of the development approval processes and traffic impact studies prepared by individual applicants.

Section 3 of the Tertiary Plan provides detailed information on the various road typologies that make up the Trafalgar Secondary Plan area. The Tertiary Plan provides information on the right-of-way elements (including width of roads, location and type of active transportation facilities), and the location of key gateway character areas.

2.3.3 Community Facilities

Community facilities include parks, trails, open space, and schools. They are distributed in a manner that allows access to all residents in the Trafalgar community, and are accessible through trails, transit and vehicles. The Agerton and Trafalgar Secondary Plans Community Facilities and Human Services Impact Analysis Report (2019) by MGP and updated letter completed by SGL details the type and size of community facilities needed for the Trafalgar Secondary Plan area.

2.3.4 Neighbourhood and Local Centres

Neighbourhood centres provide residential units, shops, services, jobs and health facilities in mixed use or standalone buildings or areas. They are intended to be major focal points for the Secondary Plan, and have the greatest intensity of uses, height and density within the Secondary

Plan area. These Centres shall have greater height and density than their immediate surroundings, being occupied by tall and mid-rise buildings. Higher densities in Neighbourhood Centres support transit, and are generally located at the intersection of arterial roads and gateways into the Trafalgar community. They contribute to a complete community and provide opportunity for jobs and residents' daily needs.

Local Centres, also identified in the Trafalgar Plan, provide convenience retail and mixed uses with lower densities than a Neighbourhood Centre and are located at the intersection of collector and arterial roads within walking distance of transit stops. Similar to the location of community facilities, the final placement and built form structure of neighbourhood centres will be guided by the Tertiary Plan and determined through the development process.



2.4 The Tertiary Concept Plan

The Tertiary Concept Plan (**Figure 6**) provides a vision for the Trafalgar Secondary Plan. It illustrates anticipated land uses, road configurations and layouts, locations for schools and parks, stormwater management ponds, and an extensive trail network centred around the community's strong central spine. The following describes key features and land uses within the Plan:

- N1** Neighbourhood Centre Mixed-Use I is an area intended to serve as a community focal point and include the retail and commercial needs of the surrounding neighbourhood, as well as a variety of built forms including street townhouses, stacked townhouses, back-to-back townhouses, multiplexes and apartments. Both stand-alone or mixed-use buildings are permitted as part of these lands.

P1 Park Type 1 (P1) are destination, centrally located public parks that provide a variety of active sports facilities. Co-location of the northerly park with the proposed secondary school provides an area for a community hub and focal point to the community. Co-location of the southerly P1 park with an elementary school and close proximity to Neighbourhood Centre Mixed Use I provides a second focal point for the community. Both parks include connections to the trail system that provide a connection to the strong central spines and throughout the community.
- N2** Neighbourhood Centre Mixed-Use II are mixed-use areas that provide for at grade retail, mixed uses and a variety of built forms including street townhouses, stacked townhouses, back-to-back townhouses, multiplexes and apartments. This mix of uses helps to promote a live work community. These centres can be at the centre or edge of a neighbourhood.

P2 Park Type 2 (P2) are local parks to serve the surrounding neighbourhood. These lands include some sports facilities and areas for open play.
- M1** These areas represent the Medium Density Residential I lands and consist of a range and mix of housing types including single-detached, semi-detached and townhouses up to 4 storeys in height. These lands provide transition between Medium Density Residential II designations and Neighbourhood Centres to the Low Density Residential designations.

VS Village Squares are small parkettes providing some recreational opportunities for residents and visitors, while promoting a sustainable green space. These spaces are animated to support continued activity on the street and to engage the community through the provided amenities.
- M2** These areas represent the Medium Density Residential II lands and consist of mid-rise multiple attached residential units such as street townhouses, stacked townhouses, back-to-back townhouses, multiplexes and apartments.

T The conceptual Trail System contributes to the active transportation network and provides access to the central spines and Natural Heritage System. This system can be used for both active transportation and recreation purposes. The trail system provides a sense of connection throughout the entire Secondary Plan area and promotes a walkable active community. Trails will be finalized through detailed draft plans.
- MU** Medium Density Mixed Use areas are generally located within Local Centres as delineated on the Community Structure Plan of the Secondary Plan. This designation permits, in a stand-alone or mixed use building format, Medium Density Residential I land uses (except single and semi-detached units), retail uses, pharmacy, financial institution, personal service uses, or small scale community uses.

TH Trailheads are wayfinding and gateways that act as entrance and decision points into and along the trail system. They can act as meeting spots for community members and markers within the system for directions and distance to destinations. The design of the trailheads can promote the character of the community and act as public art. Trailheads will be finalized through detailed draft plans.
- LC** Local Centres are areas within a neighbourhood, either at the centre or at the edge, provided to accommodate service retail commercial, co-location of neighbourhood parks, schools, and places of worship. Located at the intersection of key collector roads, these Local Centres are intended to be a focus of more intense grade-related housing within walking distance of local transit and a mix of uses including convenience retail appropriate for local resident needs.

 Existing Place of Worship

 The Secondary School is located centrally in the northern portion of the secondary area. The co-location of the Secondary School with a P1 Park provides synergies for the efficient use of land by providing opportunities to share parking, play fields and indoor facilities.
- I** These lands represent a demonstration of an interim commercial development. This area is designed to allow future streets and blocks to be constructed and the built form to re-develop and evolve over time.

 Elementary Schools are located centrally in neighbourhoods throughout the Tertiary Plan. The co-location of Elementary Schools with a P1 and P2 Parks provides synergies for the efficient use of land by providing opportunities to share parking, play fields and indoor facilities.
- L** These areas represent the Low Density Residential lands and consist predominantly of ground-related housing types such as single-detached and semi-detached, as well as street townhouses.

CO Co-location of schools and parks provide both opportunity for a neighbourhood focus as well as efficient use of lands for the community to use.
- LR** Examples of Lane based Residential providing a more urban built form and streetscape.

 Stormwater Management Facilities are located throughout the community to help manage both quantity and quality of stormwater. These areas are designed to help promote a sustainable green space in the community and link to the natural heritage system to provide green linkages across the community.

3 Community Vision

“...the function of a good visual environment may not be simply to facilitate routine trips, nor to support meanings and feelings already possessed. Quite as important may be its role as a guide and a stimulus for new exploration.”

-Kevin Lynch

The Tertiary Plan is based on community design or neighbourhood planning that encompasses aesthetics, affordability, accessibility, collaboration, communication, the opportunity to experience nature and knowledge sharing all with the intent to design and build a successful community. The foundation or structure of community design for the Tertiary Plan is critical to its overall success. This chapter provides how the vision is reflected in the structure of the community and a description of each of the structural components (NHS, Parks and Open Space, Transportation Network, Neighbourhood Centres, and Local Centres) that make up the Tertiary Plan.

A rationale for how the Tertiary Plan design reflects the Secondary Plan policies and the Trafalgar Secondary Plan Urban Design Guidelines (UDG) is provided below.

3.1 Strong Central Spines

As described in Chapter 2, the central spines of the community not only help inform the vision but also provides structure and aid in connecting the community through the NHS, complementary land uses such as parks and schools, trails, streetscapes and MUPs.

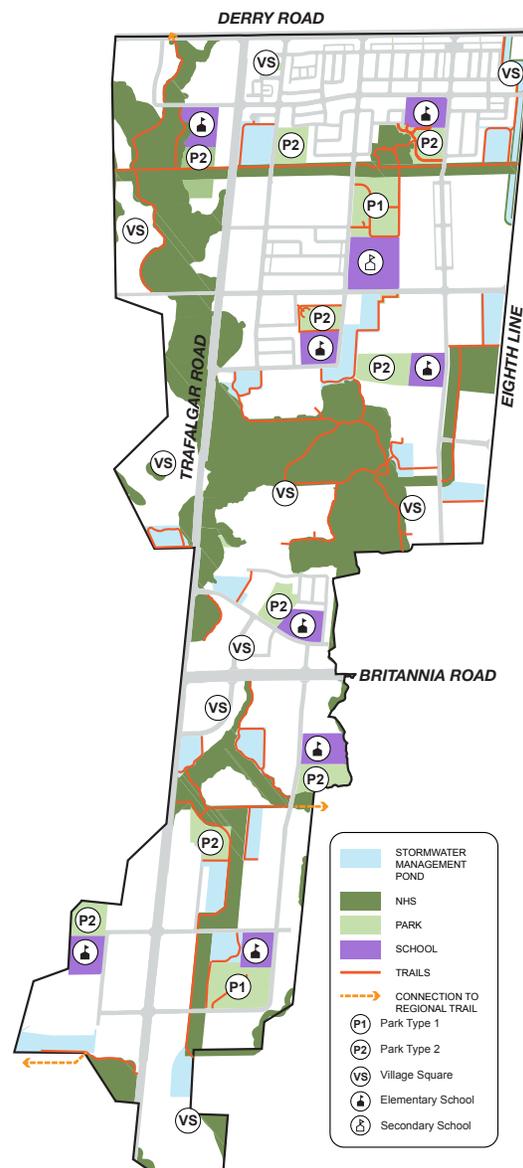


Figure 7. Complimentary land uses organized around the NHS help to create strong community spines

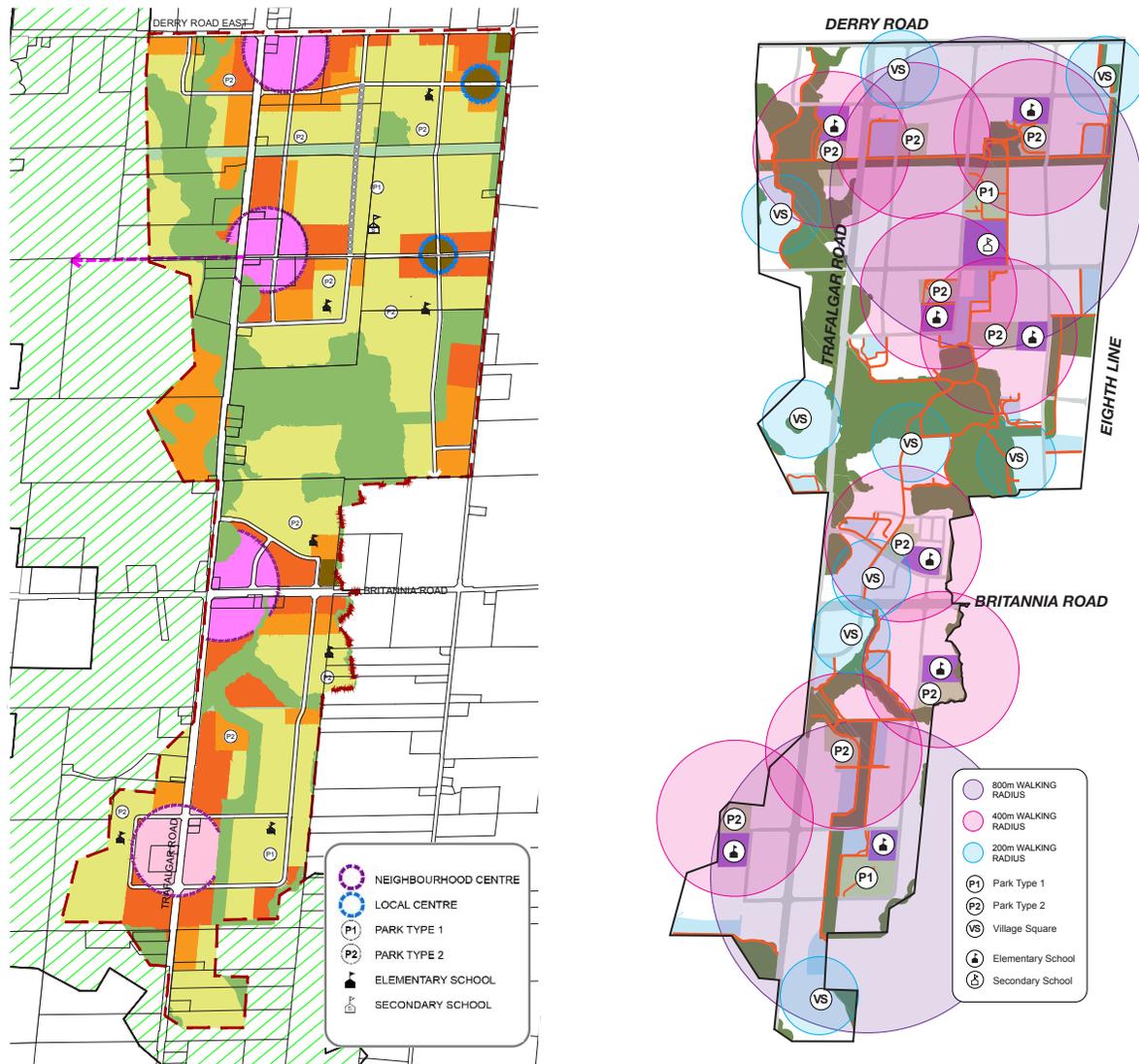


Figure 8. Trafalgar Secondary Plan (left) and Trafalgar Tertiary Plan (right) distribution of parks, community centres and Village Squares

3.2 Natural Heritage System

The extensive NHS helped inform and organize the overall design of the Trafalgar community. As illustrated in **Figure 7** and **Figure 8**, complementary land uses that include parks, schools, trails, stormwater ponds and open spaces make up the central spines and connections throughout the community. These destinations are generally distributed within a 2-minute (200m walking circle), 5-minute (400m walking circle) and 10-minute (800m walking circle) walk from each other. These connections and destinations complement the NHS and are focal points that contribute to the health and well-being of the Tertiary Plan area.

The NHS as described in the Trafalgar Secondary Plan outlines a systems approach to protecting and enhancing natural features including significant habitats, linkages, watercourses, or wetlands. The permitted uses in the NHS include non-intensive recreation uses, forest and wildlife management, and essential transportation and utility facilities. The development and connections of trail systems within the NHS creates an interconnected trail network.

The polices also allow for boundaries to be refined where supported by a MESP, a DAEFSS or an EIA or equivalent studies through approval processes under the Planning Act. The proposed Tertiary Plan

generally follows the Town’s Secondary Plan referred to in Schedule C.11.C of the Trafalgar Secondary Plan (**Figure 8**).

The Trafalgar Secondary Plan directs how the NHS is to be integrated with settlement areas to preserve and enhance biological diversity and ecological functions.

The NHS depicted in the Tertiary Plan is the result of site-specific studies for the SWS and MESP. This has included comprehensive ecological studies and feature staking to identify natural heritage feature and NHS boundaries. The NHS includes natural heritage features and natural hazards as defined by the Region of Halton and Town of Milton OPs. This includes Key Features, buffers, linkages, enhancement areas, watercourses, floodplains and wetlands determined not to be significant. Key Features in the Trafalgar Tertiary Plan NHS include significant habitat of endangered and threatened species, significant wetlands, significant woodlands, significant valleylands, significant wildlife habitat and fish habitat. It also includes appropriate buffers that, when coupled with

other avoidance and mitigation measures incorporated into the community design, will ensure the protection of Key Features in the NHS.

The total Tertiary Plan NHS is approximately 152 ha in size encompassing approximately 23% of the Tertiary Plan area. The Tertiary Plan NHS connects to adjacent very large NHS areas to the west, east and south, including Greenbelt areas to the west and south of the Tertiary Plan area. The NHS will be implemented in accordance with the recommendations of the MESP and DAEFSS.

Figure 7 and **Figure 8** shows an extensive conceptual trail network that connects various types of park and open space systems throughout the Tertiary Plan area with additional connections to external Regional trails. Environmental features are protected and enhanced through the open space layout that builds upon the key organizational role of the community’s central spines with parks, open spaces, trails, and similar compatible facilities being central to the plan.



Figure 9. An Enhanced Floodplain Area will provide stormwater runoff retention capabilities, habitats for animals and trails

3.3 Parks and Open Space System

The Parks and Open Space System consists of greenspace, Park Type 1 (P1), Park Type 2 (P2) and Village Squares and a trail network. Parks and greenspace are described here. The trail network is detailed as part of the active transportation network.

Policy C.11.3.1 e) directs the development of an interconnected system of parks and open spaces, with the NHS and public realm organized by an active transportation network. The design of the community includes spines of trails and NHS as well as connection to parks, open space and stormwater management ponds.

These elements are interconnected by a robust active transportation network that includes sidewalks, MUPs and on street bike lanes.

Policy C.11.3.2.c) encourages visibility of the NHS through the location of parks and open spaces near to or adjacent to the NHS. **Figure 10** illustrates numerous locations throughout the community where NHS is visible from various parks and trails.

3.3.1 Park Types

The Secondary Plan outlines a variety of park types to be included in the creation of a parks and open space system within the community. Various parks and open spaces are located and connected throughout the community including: Park Type 1 (P1), Park Type 2 (P2) and Village Squares.

Figure 7, Figure 8 and **Figure 12** all illustrate a connected proposed parks and open space system. When compared to the Secondary Plan, the Tertiary Plan closely follows the parks and trail locations. The following describes relevant policy

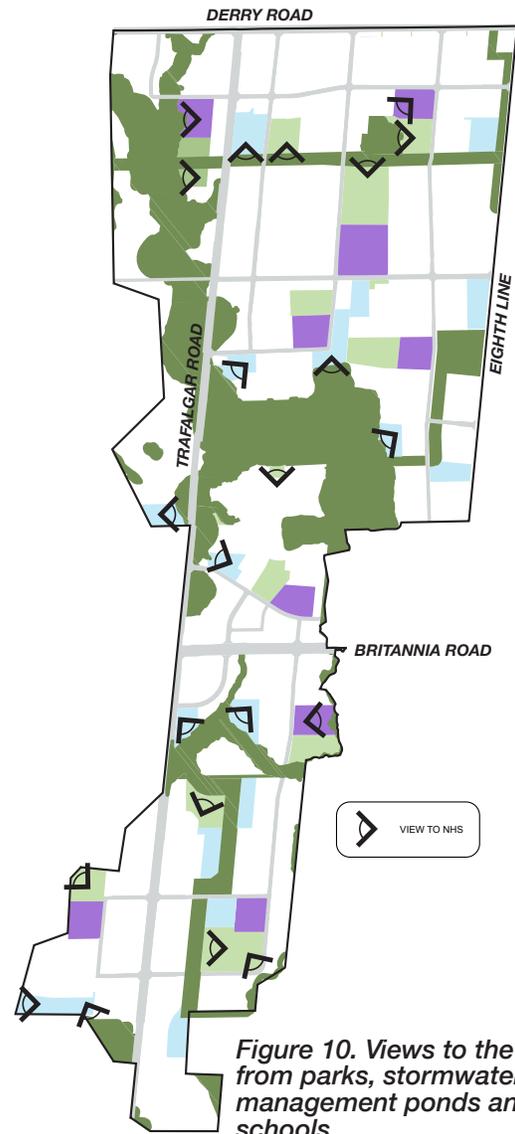


Figure 10. Views to the NHS from parks, stormwater management ponds and schools

regarding the development of parks for the Trafalgar community, and how the Trafalgar Tertiary Plan meets the intents of the Secondary Plan.

The Tertiary Plan provides a conceptual layout of P1 and P2 park facilities to demonstrate the broad range of design options that can be accommodated to meet the Town's outdoor recreation service level within the Tertiary Plan. The conceptual facility fits are not intended to restrict what will be provided within each park block.

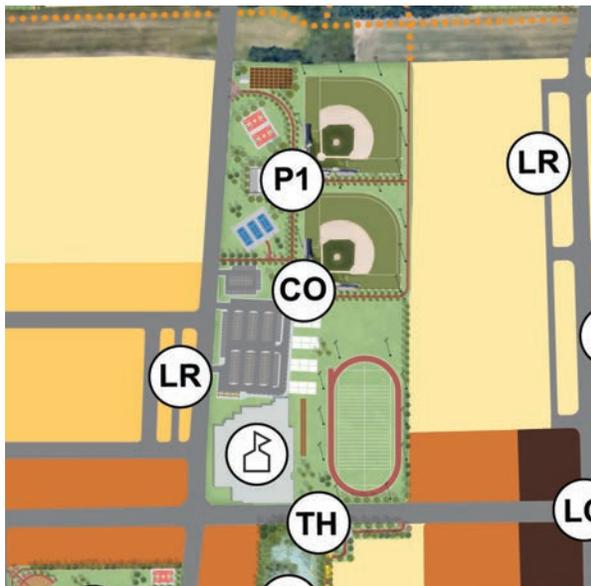
The ultimate configuration of park facilities will be determined through subsequent development processes, in consultation with the Town, based on the following principles:

- Generally, regular in shape and allow the orientation of sports facilities in a north south orientation to the extent feasible while considering the overall layout of the community road network and adjacent land uses;
- Accommodate pedestrian through-traffic and, where appropriate, cycling;
- Provide facilities for both active recreation and passive activities;
- Accommodate programmed and non-programmed activities that reflect multi-generational and multi-use needs;
- Incorporate a suitable balance of hard landscaped, soft landscaped and **designated non-programmed open space and playground**;
- Incorporate green infrastructure and low maintenance or no mow areas; and
- Accommodate where co-located with a school an appropriate program and sharing of facilities such as parking.

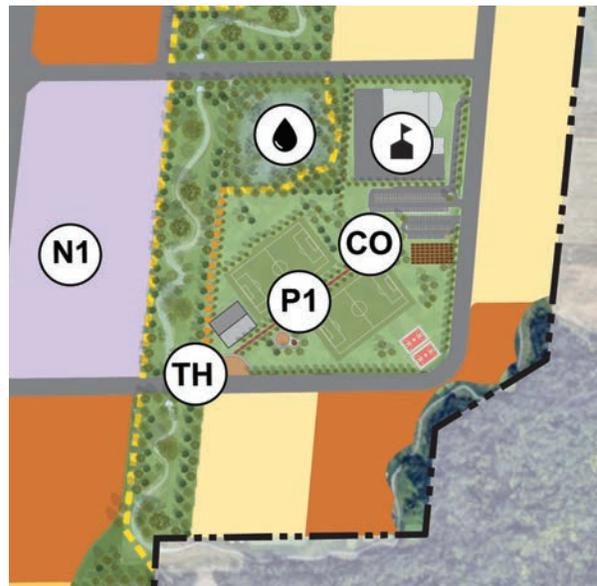
3.3.2 Park Type 1 (P1)

As per the Secondary Plan, the Tertiary Plan provides two P1 parks. These parks are a minimum of 6 ha and oriented to accommodate a range of programming such as:

- Senior Soccer or Baseball;
- Tennis/Pickle Ball;
- Multi Use Courts;
- Playgrounds;
- Community Gardens;
- Skatepark;
- Shade Structures; and
- Water play.



Northern P1 park



Southern P1 park

3.3.3 Park Type 2 (P2)

The Tertiary Plan provides nine P2 parks. These parks are generally 2.5 ha and oriented to accommodate a range of parks programming. The facility fits below demonstrate the types of facilities that could be accommodate as part of these parks.

- Shade structure;
- Soccer and/or Baseball; and
- Playgrounds;
- Multi-use courts.



Examples of P2 parks

3.3.4 Village Squares

As per policy C.11.6.3.5, a total of nine Village Squares are distributed throughout the Trafalgar community in areas that do not have good access to other park facilities. This ensures community access to parks and open space within walking distance (**Figure 11**). As described in section 3.12 of the UDG, Village Squares proposed within Neighbourhood and Local Centres will be more urban in nature. These Village Squares should be strategically located to create vibrant urban spaces. Privately Owned Public Spaces (POPS) may be provided as part of future site plans within the Neighbourhood Centres.



Figure 11. Village squares can help give a neighbourhood a sense of place.



Two P2 options for a park and school west of Trafalgar Road

3.3.5 Greenspace

Greenspace lands, as per policy C.11.6.3.7, are to provide open space connections to the NHS, generally intended for passive recreation and support the establishment of the Trail Network. These lands may include utility corridors.

There are two gas line utility corridors within the Secondary Plan area (**Figure 12**). They connect the north and the south to the central spine and trail system and provide strong east-west connections within the Plan

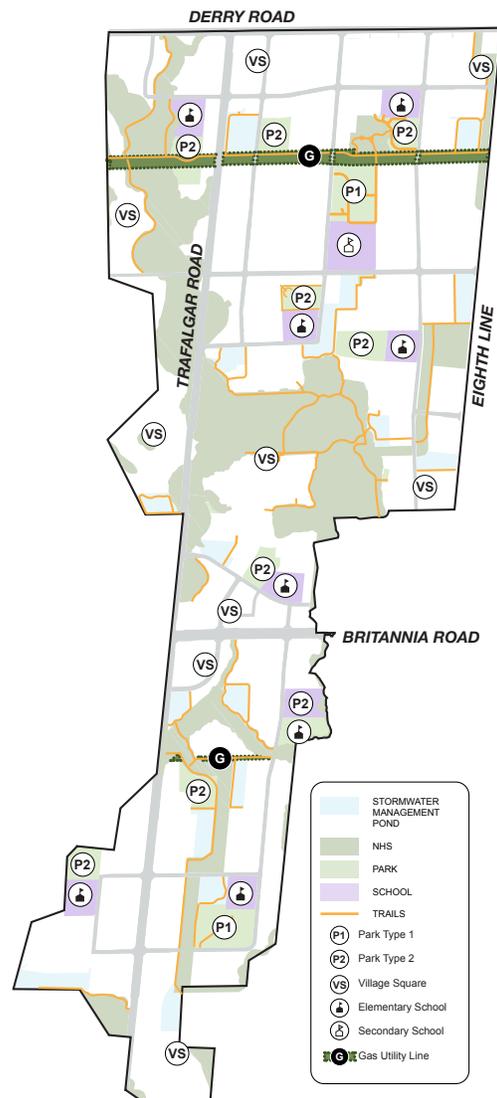


Figure 12. Gas utility corridors and trail connections

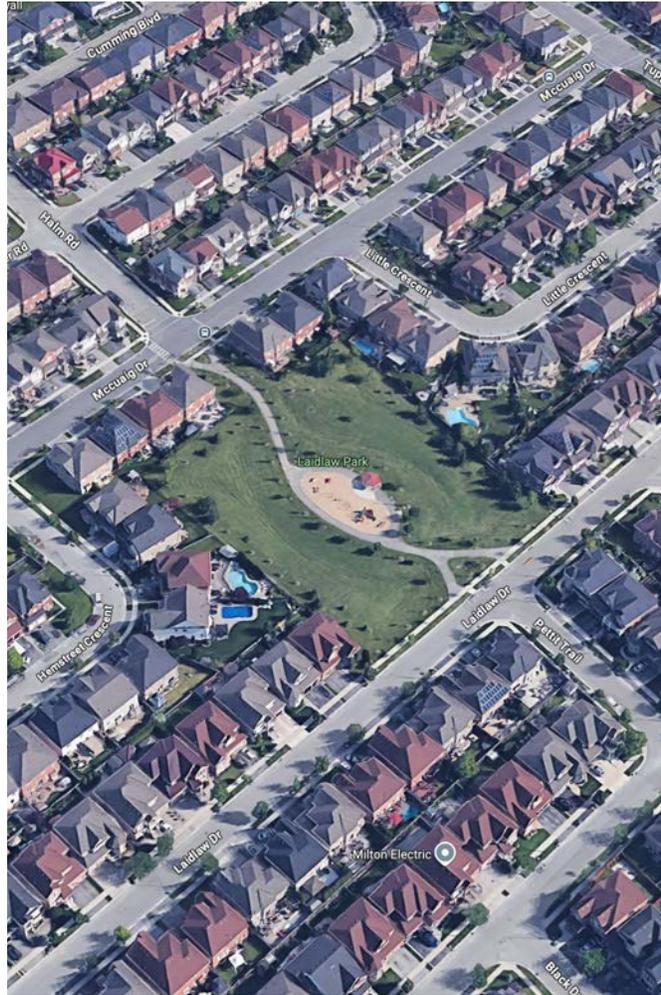
Note: Trails are conceptual will be finalized through detailed draft plans

3.4 Transportation Network

The transportation network developed as part of the Tertiary Plan supports multi-modal transportation with a variety of complete streets and an active transportation network that encourages the use of transit. The overall network is well integrated into the Town and Region’s transportation system.

3.4.1 Active Transportation

Policy C.11.2.1d) states that a connected community is a community that provides a network of “complete streets” and an active transportation and open space network accessible to all users. Active transportation consists of facilities for walking and cycling, as well as transit. Section 2.3.2 Street + Trail Networks of the UDG directs for the street and off-road network to be integrated to provide maximum connectivity. As illustrated in **Figure 13**, trails and pedestrian connections are the foundation for the development of the Trafalgar community’s design principle to connect residents to parks, open spaces and community facilities to ensure maximum connectivity and access to transit. Transit is supported through the provision of transit supportive densities, with a mix of uses and a mix of built forms adjacent to or in close proximity to transit stops. The following describes the development of active transportation facilities for the Trafalgar community, and how the Trafalgar Tertiary Plan meets the intent of the policies to provide a comprehensive network of pedestrian oriented facilities that help reduce dependence on cars and, as stated in C.11.3.3 a), “Foster a connected and accessible on- and



off-road pedestrian and cycling path network which promotes a culture of active transportation”.

3.4.2 Walking and Cycling

Policies 2.5.2.4 and 2.6.3.1 of the Town of Milton’s OP state streets and trails should be designed to increase accessibility and connect to residential, commercial, employment and recreational areas.

The Trafalgar Secondary Plan (**Figure 13**) establishes a multi-use trail system for walking and cycling. The trail system should coordinate with the existing and planned trail systems at both the Town and

Regional levels, with the siting and design of pathways to meet the satisfaction of the Town in consultation with the required approval authority. As illustrated in several figures and the Tertiary Plan, parks are encouraged to be in proximity to trails. Where trails are proposed in the NHS, the feasibility, siting and design of the trails will be subject to review based on recommendations of the MESP and/or DAEFSS.

The Tertiary Plan active transportation trail network illustrated in **Figure 13** meets the overall intent of the Secondary Plan and generally follows the same configuration as

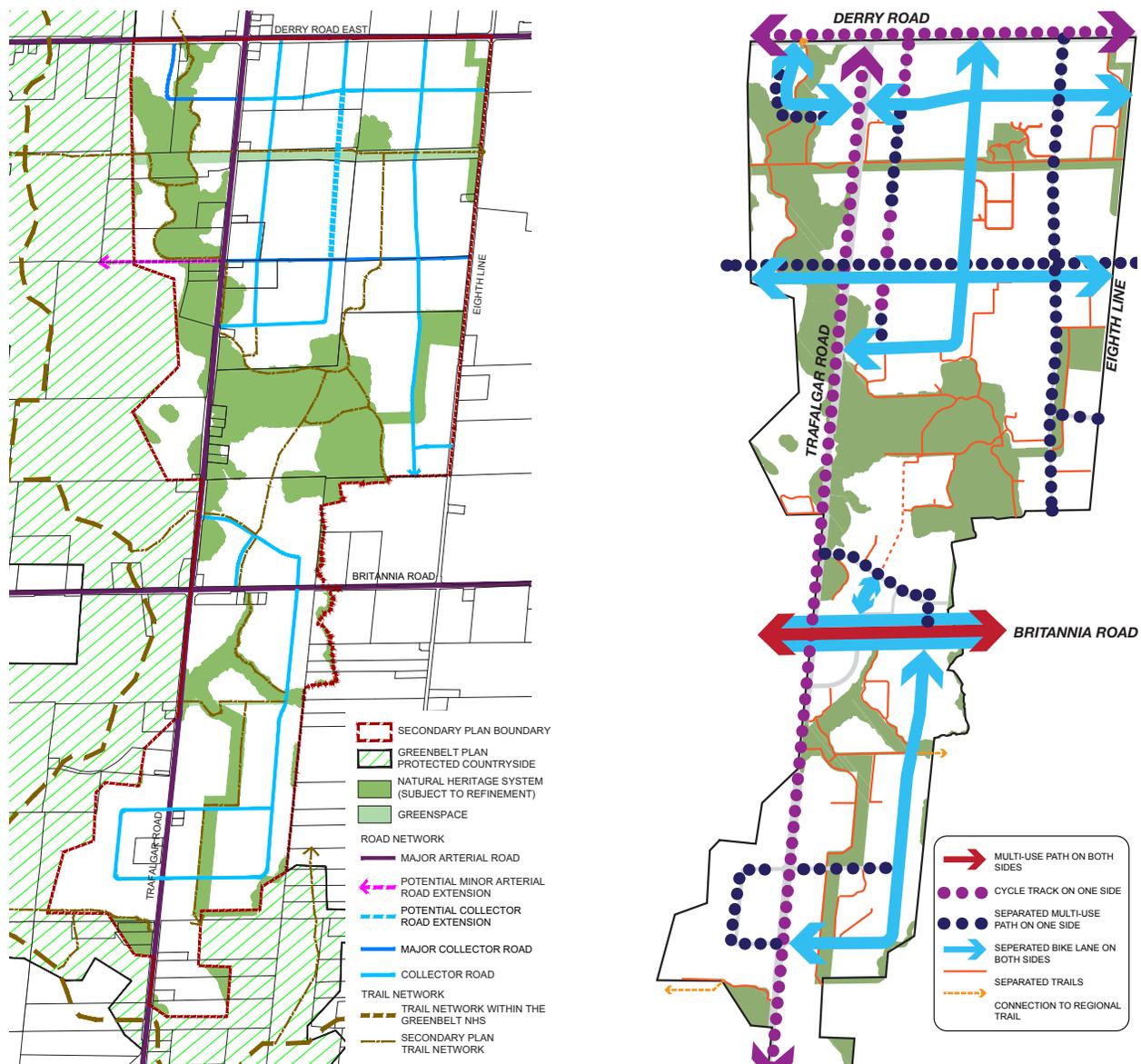


Figure 13. Trafalgar Secondary Plan (left) and Trafalgar Tertiary Plan (right) trail and bike network

Note: Trails are conceptual will be finalized through detailed draft plans

the Secondary Plan.

The proposed trails, MUPs and bike lanes contribute to a complete active transportation network by prioritizing multi-use connections to major residential, commercial, employment, and park and open space nodes throughout the Trafalgar community. Further, it connects to planned external trail systems throughout the Region to facilitate proposed connectivity and encourage walking and cycling to major destination points in the community.

3.4.3 Transit

The design of the Tertiary Plan meets the intent of policy C.11.3.3 b) to provide opportunity for local transit to support higher order transit on Trafalgar Road (**Figure 14**). Trafalgar Road will be a higher density corridor that can support the extension of higher order transit. Appendix B demonstrates walkability and connectivity of the transit network.

Policy 2.1.6.3.k) of the OP states intensification should be promoted for MTSAs and Intensification Corridors to provide increased residential and employment densities that can support transit. Overall Regional and local policy anticipates public transit along Trafalgar Road that can be supported through intensified development along this corridor.

Policies C.11.3.3 and C.11.4.2.1 of the Trafalgar Secondary Plan directs the Town to ensure the provision of transit and transit-supportive design with a focus on supporting higher-order transit planned for Trafalgar Road. This may be achieved through the provision of appropriate densities and design criteria that makes it easy for residents to access transit facilities.

local policy by situating Neighbourhood Centres, higher density mix of uses, trail connections, and transit access in close proximity to Trafalgar Road.

3.5 Road Network

The road network consists of a hierarchy of Regional Major Arterial Roads, Local Collector and Local Roads as well as Public Lanes that contribute to the area's sense of place. Policy C.11.7.5.2.f) of the Trafalgar Secondary Plan directs the Tertiary Plan to develop and provide a detailed road network assessment, including conceptual locations of local and collector roads, as well as an analysis to confirm infrastructure

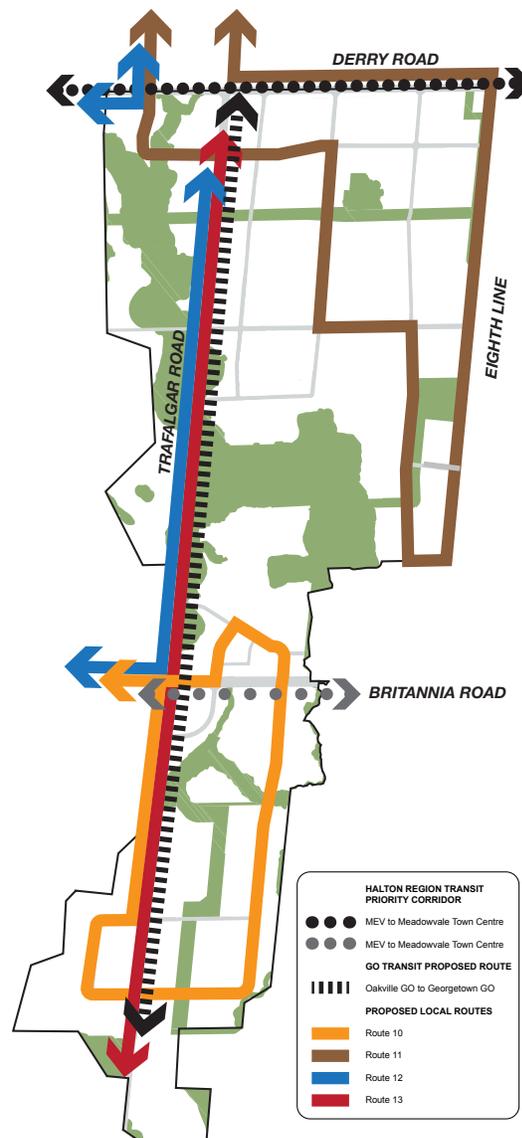


Figure 14. Transit map

The Tertiary Plan meets the intent of

requirements to accommodate full build out of development. Collector roads will ultimately be aligned to connect to the Agerton Secondary Plan road alignments, particularly at the northwest corner of the Trafalgar Secondary Plan that connects to Derry Road to allow for a full moves intersection.

Policies C.11.3.3.c and C.11.4.2.3 of the Secondary Plan directs the development of “complete streets” to balance the needs of all road users. The right road hierarchy should be placed in the right context, to allow the most logical circulation for drivers, pedestrians and cyclists. **Figure 15** illustrates principles of an efficient road network, designed as a modified grid layout as directed by the Secondary Plan Policy C.11.3.4.a and C.11.4.5.2 to facilitate walkability for this Tertiary Plan. The following describes relevant policy regarding the development of the road network for Regional Arterial Roads, Collector Roads, Local Roads, and Public Lanes.

Refer to Road Network Assessment Report, completed by TYLin for detailed roadway cross sections, and refer to the Region’s 2011 Transportation Master Plan for right-of-way requirements for Regional Roads. All right-of-way cross sections presented herein are preliminary and subject to revision through the development approvals process.

3.5.1 Regional Major Arterial Roads

Policy C.11.3.4 of the Trafalgar Secondary Plan recognizes Trafalgar Road as the main corridor for the new community, and designates Trafalgar Road, Derry Road, and Britannia Road as 6-lane Major Regional Arterial Roads (**Figure 15**). Major Arterial roads should promote active transportation, pedestrian-oriented

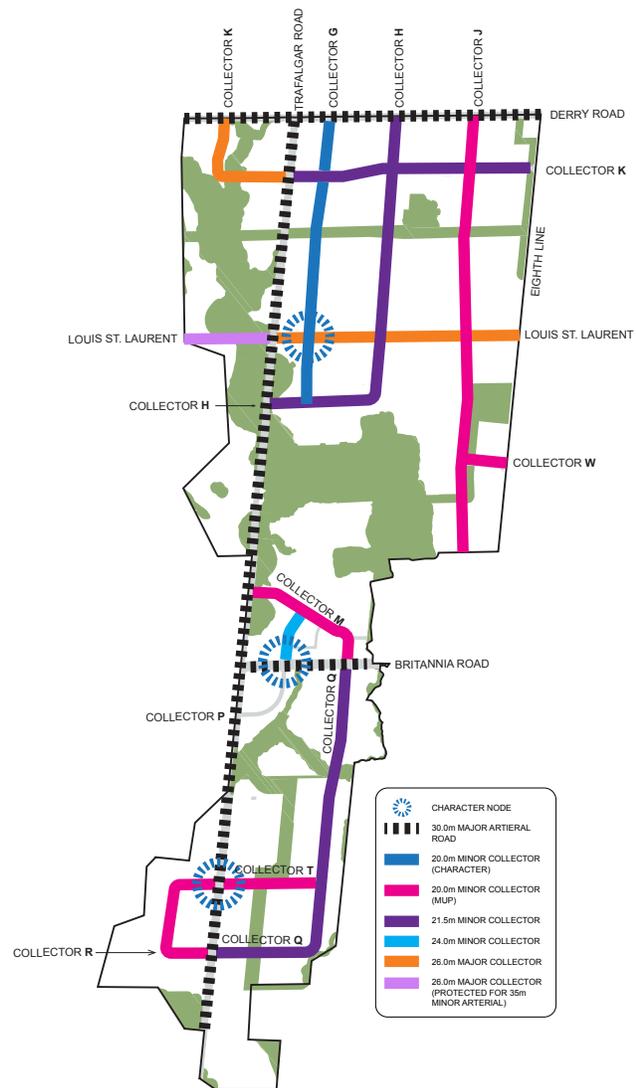


Figure 15. Trafalgar Tertiary Plan road network

development and transit-friendly facilities while maintaining the mobility function of major arterial roads.

Policy C.11.4.5.2 of the Secondary Plan directs Trafalgar Road, Derry Road, and Britannia Road to be designed with boulevards that will be vegetated with trees and shrubs, in conformance with the most up to date Regional Road Landscaping Guidelines.

As Trafalgar Road, Derry Road and Britannia Road are established Regional Arterial Roads, the Tertiary Plan does not propose any changes.

3.5.2 Collector Roads

A new east-west major collector road is identified in the Secondary Plan north of Britannia Road. Policy C.11.4.2.5 states that the continuation of this road west of Trafalgar Road will be protected for the potential future connection with the existing Milton Urban Area through the Britannia Secondary Plan and the Greenbelt. **Figure 15** illustrates a network of collector roads. The following provides a breakdown of the collector road cross sections as part of the proposed Tertiary Plan.

26 metre Right-of-Way Cross Section



The Town is proposing Louis St. Laurent Avenue and a portion of a major collector road located at the southwest quadrant of Trafalgar Road and Derry Road to have a 26 metre right-of-way. Individual driveways are not permitted adjacent to multi-use paths. These roads can accommodate buses. There is adequate space for trees with a planting trench.

This cross section includes:

- 14.7m of pavement (15.7 m curb to curb);
- 2 driving lanes;
- Bike Lanes on boulevard adjacent to sidewalk;
- Sidewalks on both sides of the street; and
- On-street parking on one side of the street.



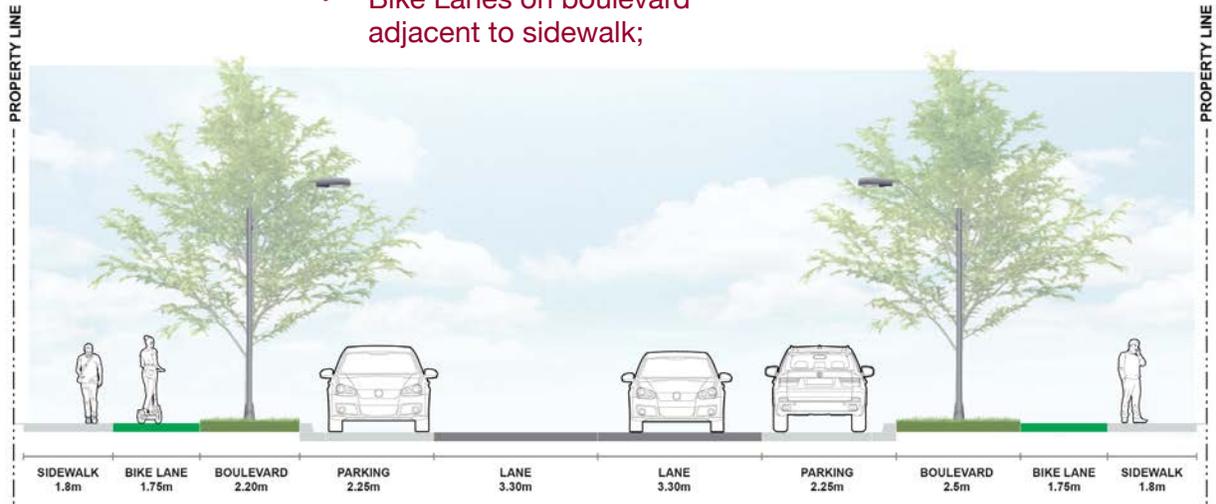
24 metre Right-of-Way Cross Section



There is one collector road with a 24 metre right-of-way. This collector road permits parking and bike lanes on both sides of the street, sidewalks on both sides of the street and accommodates buses. There is adequate space for trees with a planting trench.

This cross section includes:

- 14.5m of pavement (15.5 m curb to curb);
- Sidewalks on both sides of the street; and
- 2 driving lanes;
- On-street parking on both sides of the street.
- Bike Lanes on boulevard adjacent to sidewalk;



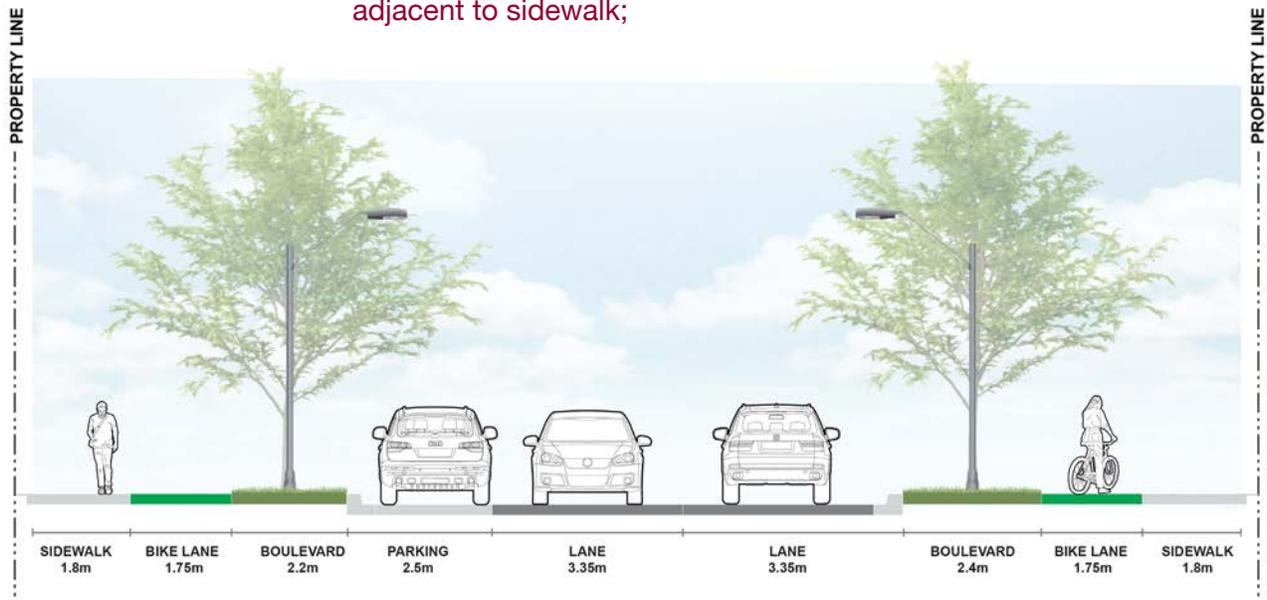
21.5 metre Right-of-Way Cross Section



There are three collector roads with a 21.5 metre right-of-way. These collector roads permit parking on one side of the street. These roads can accommodate buses. There is adequate space for trees with a planting trench.

This cross section includes:

- 12.1m of pavement (13.1 m curb to curb)
- 2 driving lanes
- Sidewalks on both sides of the street; and
- On-street parking on one side of the street.
- Bike Lanes on boulevard adjacent to sidewalk;



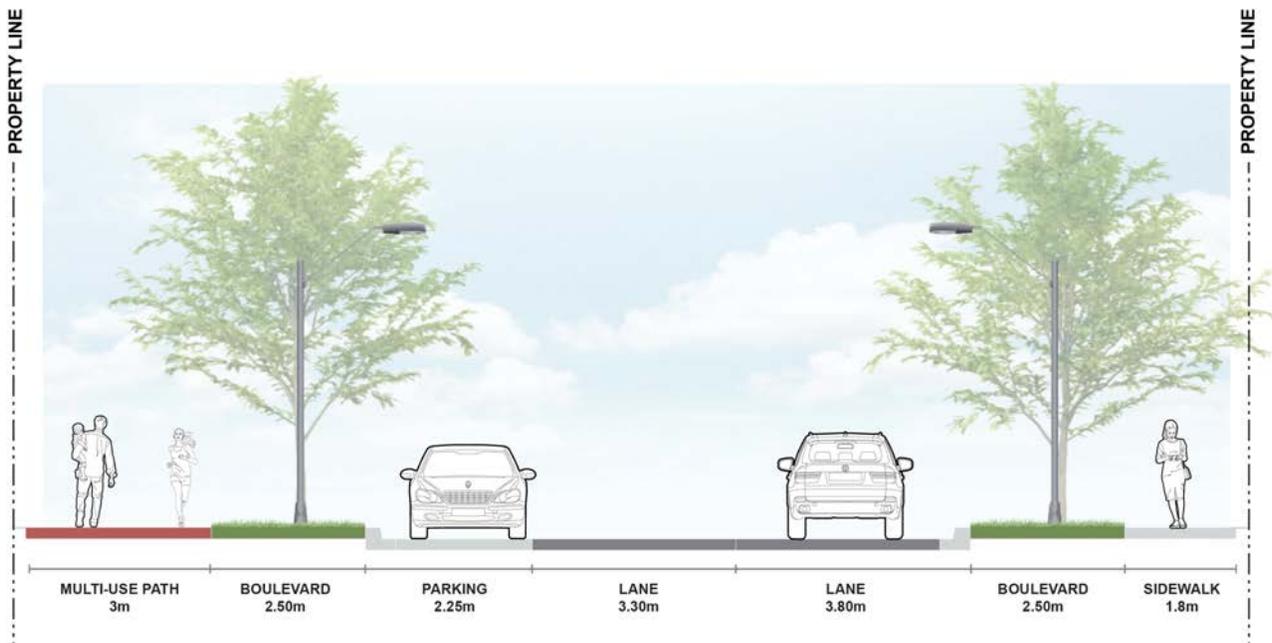
20 metre Right-of-Way Cross Section



There are five collector roads with 20 metre right-of-ways. These collector roads permit driveways, and can accommodate buses. There is adequate space for trees with a planting trench.

This cross section includes:

- 8.8 m of pavement (9.8 m curb to curb);
- 2 driving lanes;
- A sidewalk on one side of the street and a MUP the other side of the street; and
- On-street parking on one side of the street.



Example of a multi-use path in Milton



Example of a boulevard

20 metre Character Road Cross Section

Special Character Collector Road

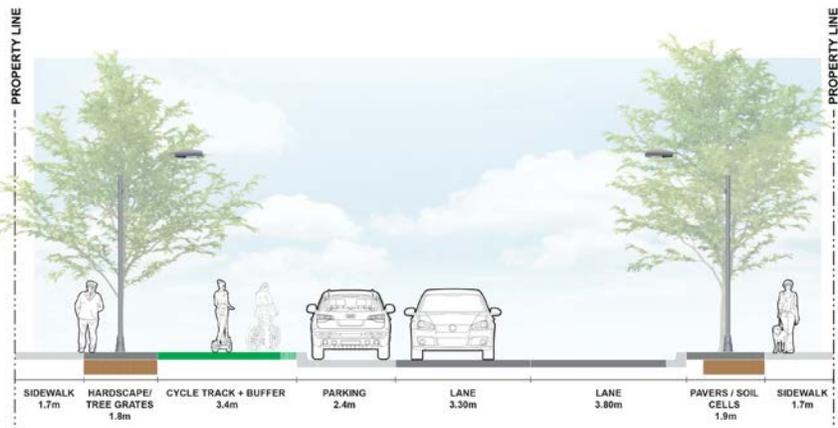


Policy C.11.4.5.2 j) states that there shall be a “Special Character” collector road, approximately parallel to Trafalgar Road. As described in section 3.5 of the UDG the “Special Character” collector road is to function as a more pedestrian oriented alternative to Trafalgar Road.

Development between Trafalgar Road and this road should reflect its active transportation focus and shall be designed to facilitate ease of east-west movement between higher-order transit stops on Trafalgar Road and the collector road. Section 2.3.3 of the UDG describes the intent of the “Special Character” collector road along its length and in response to the adjacent context. In response the Tertiary Plan proposes a 20.0 metre ROW section with 3 variations. These are described below.

The cross section within the Node includes:

- 8.8m of pavement (9.8m curb to curb);
- 2 driving lanes;
- Sidewalks on both sides of the street;
- On-street parking on one side of the street; and
- Cycle Track on one side of the street.

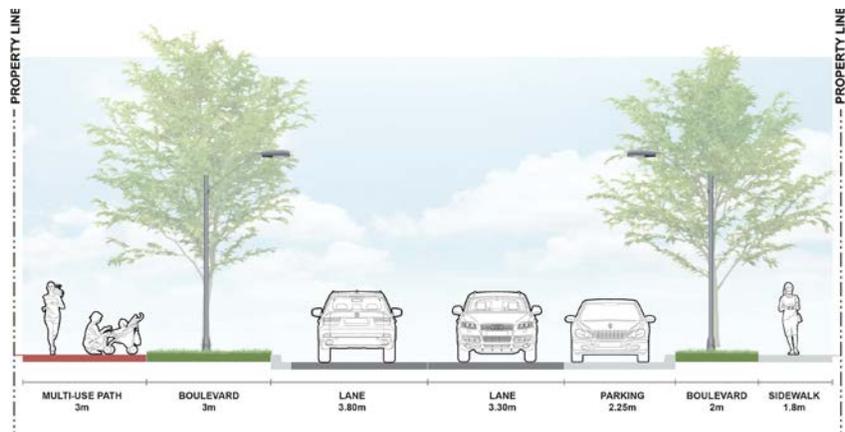


Examples of tree grates and urban boulevards

Elevated and separated bike lane, Cambridge

The cross section outside the Node includes:

- 8.5m of pavement (9.5m curb to curb);
- 2 driving lanes;
- A sidewalk on one side of the street and a MUP the other side of the street; and
- On-street parking on one side of the street.



3.5.3 Local Roads

Secondary Plan Policy C.11.4.5.2 directs local roads to promote active transportation and discourage car movement through neighbourhoods and non-residential traffic within neighbourhoods.

Appendix A provides a Demonstration Plan of one way in which local roads could be located within the Tertiary Plan. Approximately 35 linear kilometres of local roads are proposed. The local roads will be further refined through each draft plan of subdivision.

3.5.4 Laneways

The Trafalgar community includes public lanes. Public lanes are key community design components that not only reduce conflicts between pedestrians and drivers but also provide for a variety of lane based built form that is not dependent on individual driveways. The benefits of laneways for the Trafalgar community are:

- Increased density while maintaining a high quality public realm;
- Safer streetscape due to no conflicts with sidewalks and driveways;

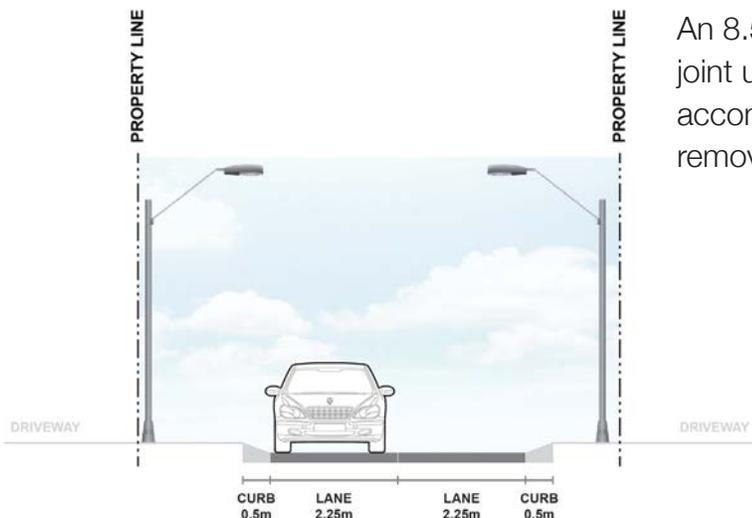
- Increased/continuous on-street parking;
- Additional space for safe recreation e.g. street hockey, basketball;
- Continuous tree planting within public boulevard along the local street to reduce heat island effect and provide adequate soil volume for tree growth;
- Improved built form with more windows and street presence; and
- Clear view of the streetscape with no interruption from garages.

3.5.4.1 Lane Location Principles

In addition to all the benefits of lane based built form, lanes may be used to reduce conflicts and create safer streets by removing individual driveways:

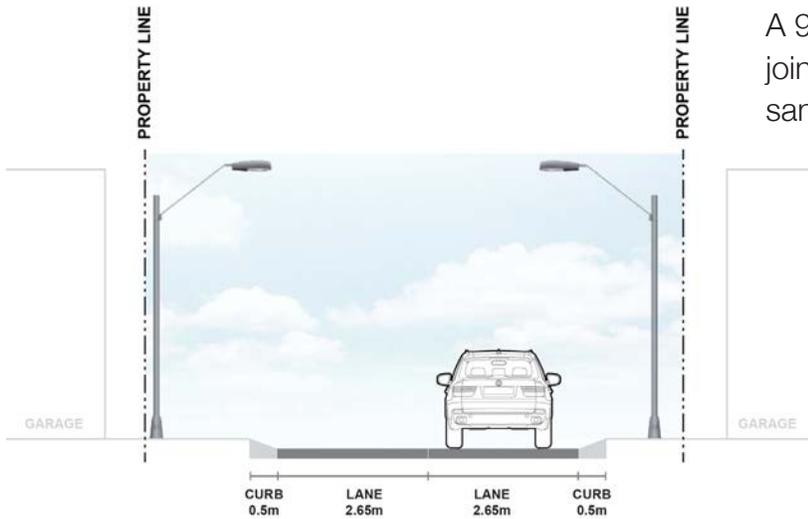
- In the vicinity of an arterial road;
- Adjacent to a MUP; and
- For townhouses less than 6.5m wide.

There are three lane cross sections proposed: 8.5, 9.5 and 10 metre laneway.



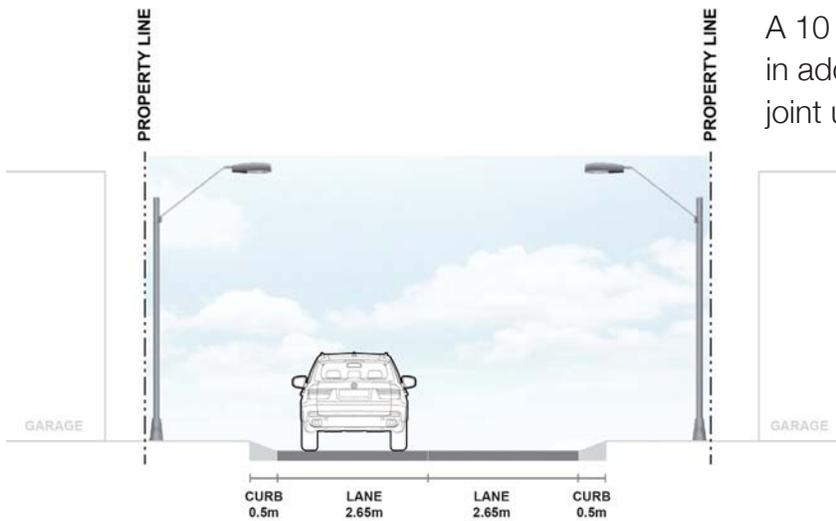
An 8.5 metre laneway can accommodate a joint utility trench and storm sewer and can accommodate garbage pickup and snow removal.

Figure 16. 8.5 metre laneway



A 9.5 metre laneway includes a joint utility trench, storm sewer and sanitary sewer and watermain.

Figure 17. 9.5 metre laneway



A 10 metre lane can accommodate in addition to a 9.5 meter lane a joint utility trench on both sides.

Figure 18. 10 metre laneway

3.5.4.2 Lane Criteria

Servicing

- Public lighting is to be provided in the lane.

Snow Removal

- Laneways are required to accommodate snow removal equipment.

Laneways are identified on the Tertiary Plan in Chapter 2.

3.6 Community Facilities

Policy C.11.7.5.2.c directs for the Tertiary Plan to address potential locations of libraries, community centers, emergency services and places of worship.

The Fire Services Master Plan identifies the preferred location for a new fire station on the north side of Derry Road within the Agerton Secondary Plan area. In addition, a community centre is being proposed in the Agerton Secondary Plan to accommodate both the Trafalgar and Agerton Secondary Plan areas.

3.6.1 Places of Worship

In addition to the one existing place of worship, 4 additional places of worship may be required. The Tertiary Plan, as directed by the Secondary Plan provides many opportunities for places of worship to be located:

- In a mixed use building of a stand alone building in a neighbourhood or local centre
- Permitted as part of an elementary school or the secondary school

3.6.2 Schools

Schedule C.11.C of the Secondary Plan provides for the number and location of schools to be provided within the plan. (Figure 19). Based on the proposed population of 45,000, the Tertiary Plan provides one secondary school and eight elementary schools. In addition to the number of schools, the Tertiary Plan provides preliminary facility fits for the proposed schools. In all cases except one, schools are paired with parks. The proposed school west of Trafalgar Road near the north end of the Secondary Plan area is adjacent to a park located directly across the street.

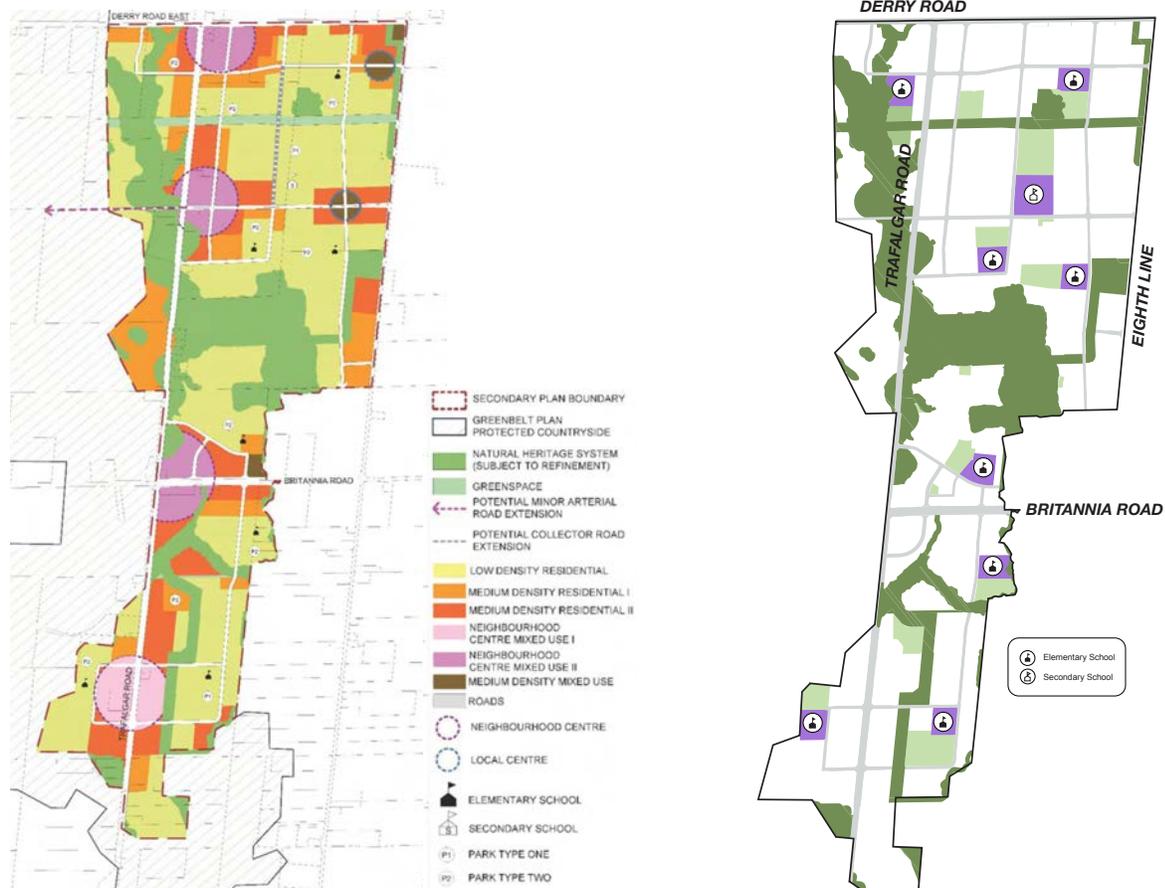


Figure 19. Schedule C.11.C of the Trafalgar Secondary Plan (left) and proposed location of schools in the Tertiary Plan (right)

3.7 Land Use Mix

The Trafalgar community consists of a variety of land use types that aim to provide diversity in land use configurations. This ensures that housing can accommodate the diverse needs of residents of all ages and incomes. Land use mix is directed by the following Secondary Plan Policies:

- Policy C.11.4.4.1 directs the provision of a mix of housing types that meet the life cycle needs of residents; and
- Policy C.11.6 of the Trafalgar Secondary Plan provides direction for various land use requirements. Details for proposed density, number of units by type and mix can be found in Chapter 4.



Low Density Residential

The Secondary Plan directs:

Built Form Type

Ground related – singles, semi-detached, street townhouses, back-to-back townhouses

Built Form Mix

Maximum 25% Street Towns

Density

Minimum 27 units per net hectare



Medium Density I

The Secondary Plan directs:

Built Form Type

Ground related – street, stacked and back-to-back townhouses, singles and semi-detached

Built Form Mix

Maximum 20% Singles and Semi-detached

Density

35 - 100 units per net hectare

Height

Maximum height of 4 storeys



Medium Density II

The Secondary Plan directs:

Built Form Type

Ground related – street, stacked and back-to-back townhouses, multiplexes, apartments adjacent to arterial / collector roads

Density

60 - 120 units per net hectare for townhouses and multiplexes

Floor Space Index (FSI)

Maximum of 3.0 for apartment buildings up to 8 storeys



Schools | The Tertiary Plan provides 8 elementary schools and 1 secondary school

Phase 1

5 elementary schools
1 secondary school

Phase 2

3 elementary schools

3.8 Local and Neighbourhood Centres

Policies C.11.5.3 Local Centres and C.11.5.4 Neighbourhood Centres of the Secondary Plan expand on the development of a community structure through the establishment of Local and Neighbourhood Centres as areas that should accommodate co-location of parks, schools, places of worship, retail commercial, and a range of housing types of varying heights and densities. The Tertiary Plan meets the intent of the Secondary Plan by providing a hierarchy of Centres – with Local Centres providing services for everyday needs situated on



Figure 20. Examples of built form within the Local Centres and Mixed Use lands

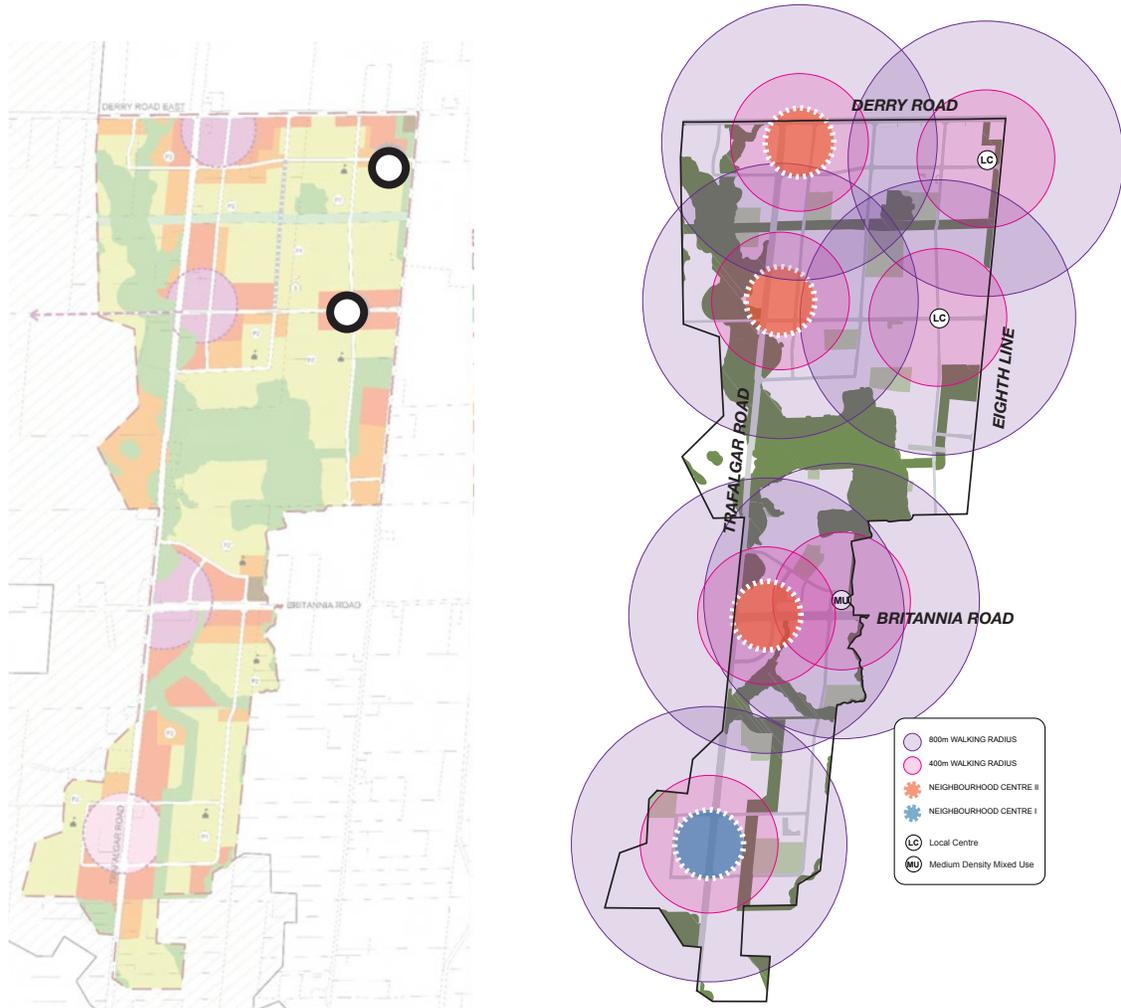


Figure 21. Trafalgar Secondary Plan (left) and Trafalgar Tertiary Plan (right) location of Local Centres

key collector roads and Neighbourhood Centres providing transit-supportive densities and land uses situated on arterial roads. **Figure 20** provides examples of Local Centres. **Figure 21** illustrates how these centres are aligned within the Trafalgar community to ensure almost all parts of the community are anchored by Local or Neighbourhood Centres.

As per the Secondary Plan, the Tertiary Plan provides more detail on location, size and configuration of each of the local and neighbourhood centres. An approximate amount and breakdown of commercial GFA is detailed in chapter 4 as part of each phase and stage.

3.9.1 Local Centres

The UDG describes the Local Centres as minor activity centres with convenience retail and service related to the surrounding neighbourhood. Two Local Centres are situated in the Tertiary Plan located at the intersection of key collector roads to ensure walkability for the surrounding residents (**Figure 22** and **Figure 23**). All centres propose the required mix of residential and commercial uses. Refer to Chapter 4 for Local Centre statistics.



Figure 22. Northeastern Local Centre

A Local Centre along a collector road, south of the corner of Derry Road and Eight Line proposes both stand-alone commercial and commercial at-grade (**Figure 22**) as part of a mixed use building. A dot indicates the location of stand-alone commercial and a red line indicates the approximate location of commercial at-grade. This Local Centre function is further enhanced with a Village Square, a realigned naturalized channel and a trail connection to be able to walk or bike between both areas.

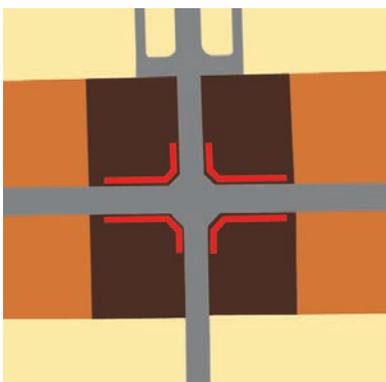


Figure 23. Central Local Centre

The second Local Centre is located at the intersection of Louis St. Laurent Avenue and a new collector road. This mixed use centre includes commercial at-grade. **Figure 23** shows the proposed commercial frontage within each building. This Local Centre is located close to the proposed secondary school and P1 park, a trail head that accesses the spine of the community and an MUP along Louis St. Laurent Avenue.

The proponent must provide the estimated gross floor area of non-residential land uses at the time of submitting a development application.

3.9.2 Neighbourhood Centres

Policy C.11.5.4 of the Secondary Plan states:

Neighbourhood Centres are intended to be major focal points for the Secondary Plan, and shall have the greatest intensity of uses, height and density within the Secondary Plan...each is intended to provide a full mix and range of uses at transit-supportive densities between 100 and 160 residents and jobs per hectare.

Section 2.3.4 Neighbourhood Centres of the UDG describes these as major activity centres that are urban in nature. As with the Local Centres, the Secondary Plan directs for the Tertiary Plan to provide more details on the location, size and configuration of each of the Neighbourhood Centres.

The Tertiary Plan situates Neighbourhood Centres in the same location as identified in the Secondary Plan. There are two types of Neighbourhood Centres (**Figure 24**) detailed below. Refer to Chapter 4 for Neighbourhood Centre statistics.

Located south along Trafalgar Road between two new collector roads, the proposed Neighbourhood Centre Mixed-Use I is intended to develop at a lesser density than that of Neighbourhood Centre Mixed-Use II. The design vision of Neighbourhood Centre Mixed-Use I is to establish a unique sense of place and distinct neighbourhood with a mix of uses in stand-alone and mixed use built form that is connected to the surrounding community through trails to encourage walkability. Secondary Plan Policy C.11.6.2.2.i) directs this node to have between 9,000 - 14,000m² of neighbourhood retail that will be anchored by a larger tenant such as a food store or pharmacy, institutional uses and a minimum of 100 residents and jobs per gross hectare. **Figure 25** shows the proposed location of commercial frontage with a red line. The mix of housing types includes mid-rise multiple attached residential units such as street townhouses, stacked townhouses, back-to-back townhouses, multiplexes and apartments up to 15 storeys high.

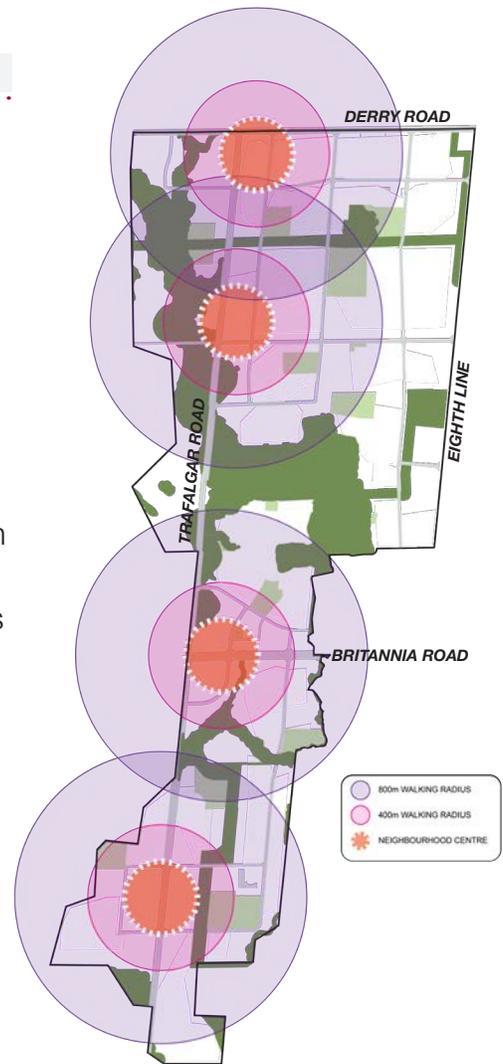


Figure 24. Distribution of Neighbourhood Centres



Figure 25. Neighbourhood Centre Mixed-Use I area

Neighbourhood Centre Mixed-Use II designation is intended to be the focus of intensification within the Trafalgar community. These Centres are located at the intersection of Derry and Trafalgar Roads, Britannia and Trafalgar Roads, and the intersection of Louis St. Laurent Avenue and Trafalgar Road (**Figure 26** and **Figure 27**). Complementary land uses and densities surround Neighbourhood Centres as illustrated in **Figure 28**.

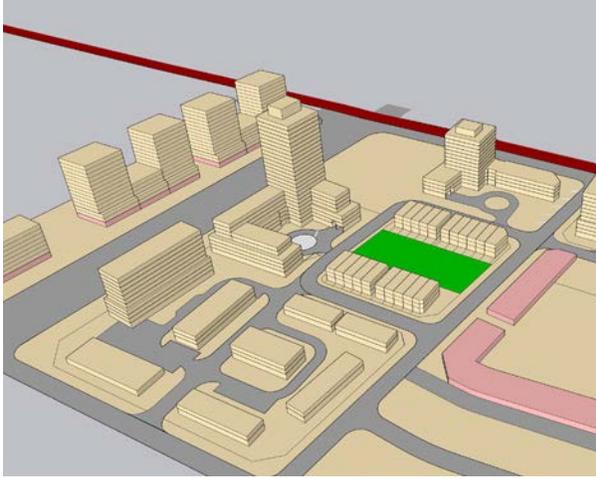


Figure 26. Built form at the intersection of Derry and Trafalgar Roads shall orient to the street



Figure 27. Neighbourhood Centre Mixed Use II along Britannia Road

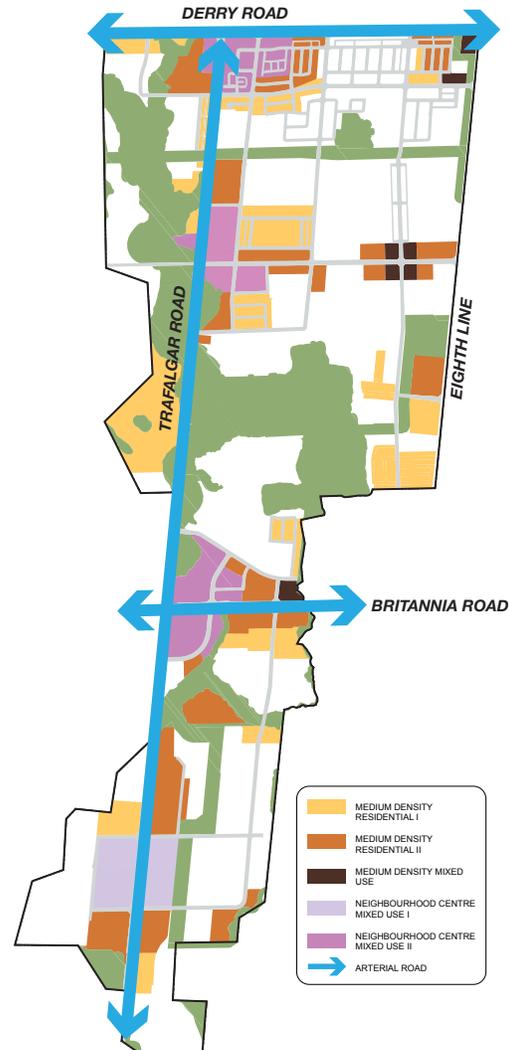
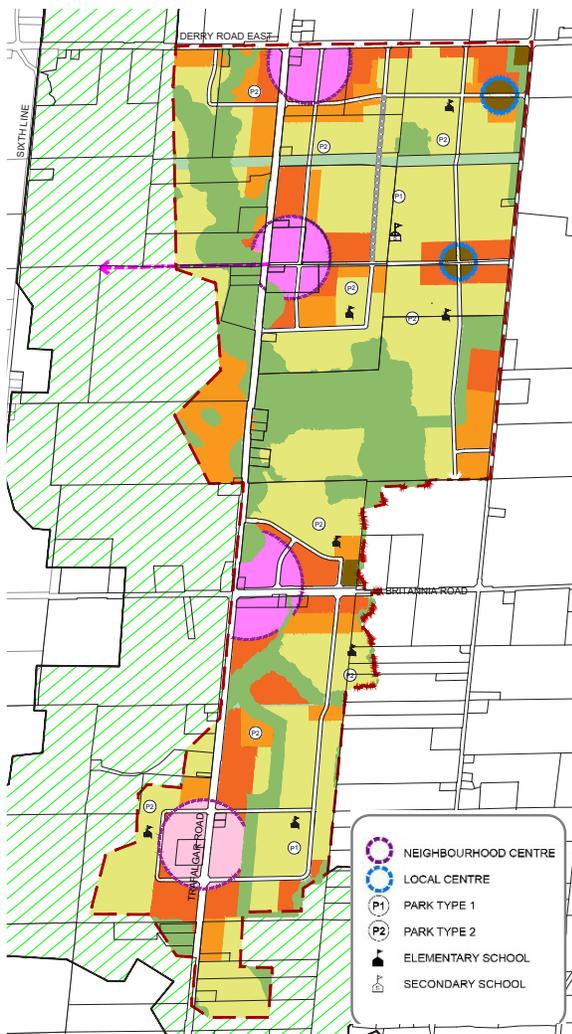


Figure 28. Trafalgar Secondary Plan (left) and Trafalgar Tertiary Plan (right) distribution of densities along Trafalgar Road, Britannia Road, Eighth Line and Louis St. Laurent Avenue



Figure 29. Derry and Trafalgar Road Neighbourhood Centre

The Derry and Trafalgar Road Neighbourhood Centre (**Figure 29**) is envisioned to contain a mix of built form types and land uses with a walkable main street / “Special Character” road and Village Square central to this node. Land uses shall ensure the daily needs of residents is available within a 5- to 10-minute walk. Stand-alone interim commercial uses are planned to redevelop over time, with a preliminary street layout that considers future streets and blocks to develop over time.



Figure 30. Neighbourhood Centre located along Trafalgar at Louis St. Laurent Avenue

The Neighbourhood Centre located along Trafalgar Road and Louis St. Laurent Avenue will have its own sense of place while still satisfying the daily needs of the residents and employees by providing goods and services in the form of commercial at-grade in mixed use buildings or interim stand-alone buildings. **Figure 30** shows the location and configuration of the commercial at-grade.



Figure 31. Britannia and Trafalgar Road Neighbourhood Centre

The Britannia and Trafalgar Road Neighbourhood Centre is envisioned as the central node with the Trafalgar community, located at the most central intersection that extends to the broader Region. This node proposes heights up to 25 storeys with a mix of mixed use mid-rise and tall buildings that maintain active frontages for pedestrians. Residents will be able to access transit along Trafalgar and Britannia Road with mid-block connections through buildings. **Figure 31** provides the proposed location of commercial at-grade.

4 Phasing & Implementation

“There is no power for change greater than a community discovering what it cares about.”

-Margaret J. Wheatley

This exciting new community will be developed in two phases per the Secondary Plan, with a proposed population of 45,000 and 17,260 units by 2051. This chapter describes the staging of development within each Phase. With the goal of creating or contributing to a complete community, at each stage within the phases, community amenities such as parks and trails, school sites, stormwater management ponds and commercial areas for daily needs are proposed to be delivered.

Policy C.11.7.1.2 provides direction for the Tertiary Plan to control growth through the delivery of infrastructure and services at each stage of development. Each stage of development will support a school and park. The Tertiary Plan process is to ensure the availability and efficient use of infrastructure and services for each development stage.

To support sustainable growth of the community, the Town requires that the services and infrastructure are provided not only in accordance with applicable legislation but in a fiscally

Phase 1 and Phase 2

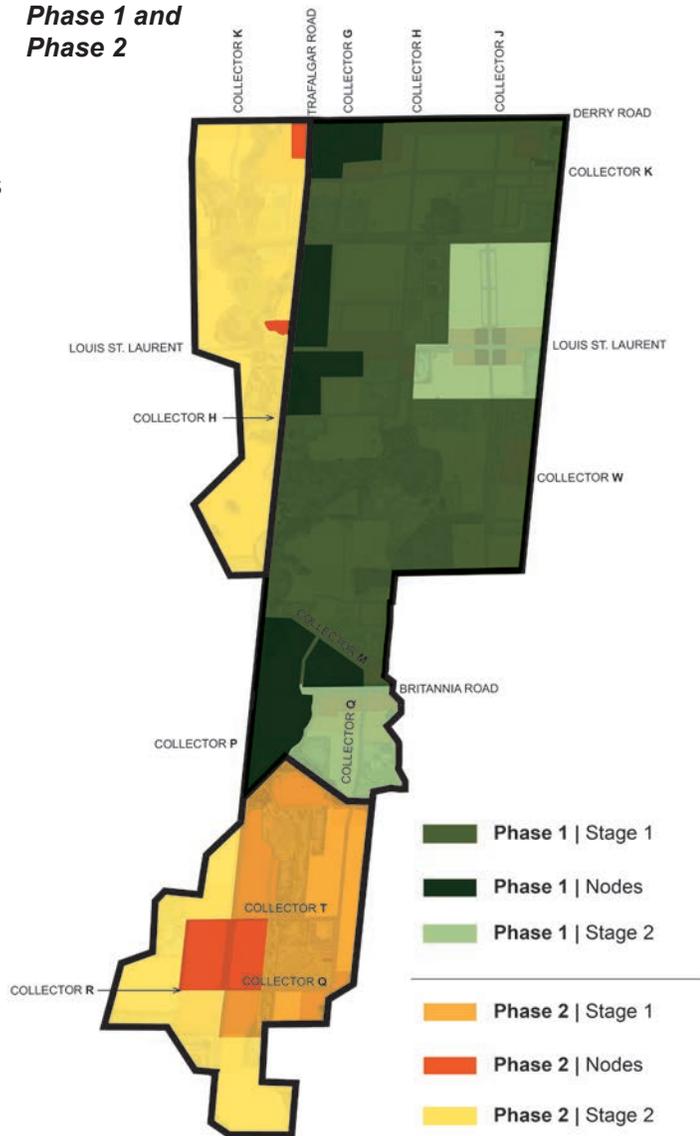


Figure 33. Phases and Stages delineated

responsible manner while meeting the Secondary Plan objectives.

As well, the policy requires there to be a range and mix of housing types in each phase. Development will proceed in a logical sequence, generally from north to south ensuring the creation of complete neighbourhoods and minimizing the extent to which future residents are exposed to construction.

In addition to the Secondary Plan policies, landowners have participated in the Region’s 2020 and 2023 Allocation Programs which, in part, funds the delivery of Regional water, wastewater, and road infrastructure for the Trafalgar lands. Participation in the allocation program is required for development to proceed and is therefore a foundational consideration in the overall staging of the Trafalgar community.

The 2020 and 2023 allocated properties form the basis for the lands included in Phase 1, Stage 1.

The Phases and Stages are delineated in **Figure 33**, in accordance with the Secondary Plan. Phase 1 is bounded by Derry Road to the north, Eighth Line to the east, the Lower Middle Tributary to the south and Trafalgar Road to the west. Phase 2 includes the lands south of the Lower Middle Tributary and West of Trafalgar Road. The Phases are divided into Stage 1, Nodes, and Stage 2.



Rendered view of proposed secondary school at Louise St. Laurent and a new collector Road

4.1 Phasing and Staging

The assumptions used to calculate population and employment numbers for each phase and stage are provided in **Appendix C**.

Tertiary Plan submission requirements, outlined in policy C.11.7.5.2, state that prior to making any application for draft plan approval, a Tertiary Plan shall be prepared in consultation with applicable agencies and submitted in accordance with the policies of Section B.5.4.3.6 and B.5.4.3.7 of the OP. The Tertiary Plan will address and demonstrate stages within each phase including a breakdown of anticipated dwellings by type including affordable housing, non-residential uses by area, associated population and employment and the delivery of roads including local roads for each phase. As per the Secondary Plan, the number of proposed affordable housing units is determined based on the number of stacked townhouses, back-to-back townhouses and apartments in each stage of development. In addition, each Phase describes the delivery of roads as per the RNA. Outcomes and recommendations of the SWS and MESP are outlined in accordance with policy C.11.7.5.2.k.

The updated RNA includes analysis for the 2031, 2041, and 2051 horizon years. A detailed description of the Staging and Monitoring Plan is provided in Section 9 of the updated RNA dated November 2025. Additionally, a detailed review of what was considered for each horizon year is detailed in Section 6 of the updated RNA.

Sections 4.2 and 4.3 of the Tertiary Plan provide a breakdown of anticipated dwellings by type including affordable housing, non-residential uses by area and associated population and employment for each stage and phase, within Local Centres and Neighbourhoods. In addition, an estimated amount of commercial GFA (see Appendix D), the number of schools, the type and number of parks, the number of storm ponds proposed, the length of trails, roads and MUPs.

The Tertiary Plan provides the general location, size and configuration of parks, schools and stormponds and gives an idea of what these areas might look like at buildout.



Rendered view of a Park Type 1

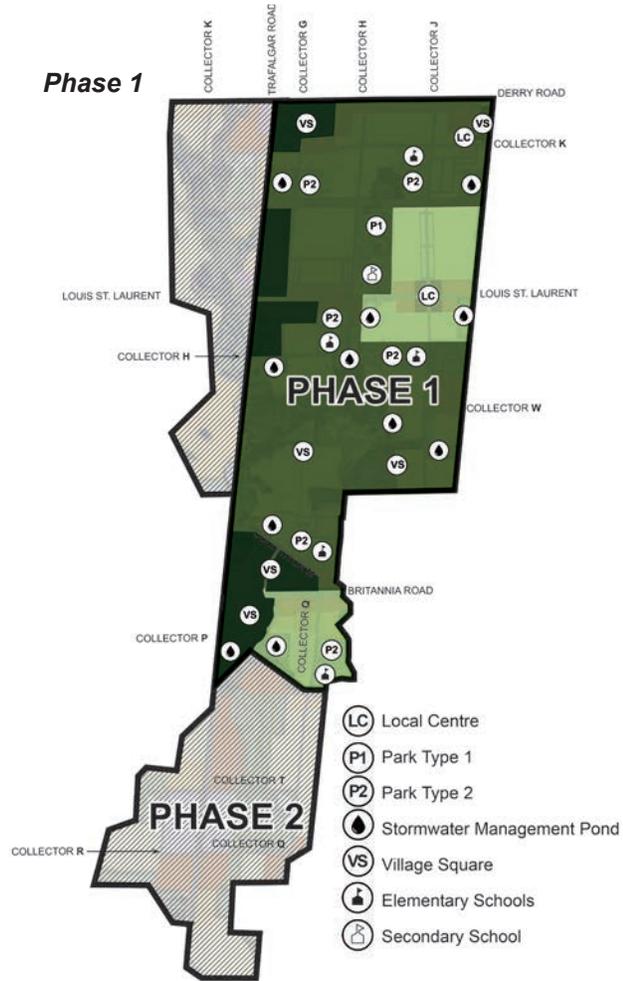
4.2 Phase 1

Phase 1 is the largest phase of development and will greatly contribute to the delivery of public infrastructure, facilities, services and amenities and will ensure the achievement of complete, healthy and sustainable neighbourhoods.

Phase 1 proposes the development of Medium Density Mixed Use areas, Neighbourhood Centre Mixed Use II areas, Local Centres, a Park Type 1 (P1), six Park Type 2 parks (P2), eleven stormwater management ponds, six Village Squares, five elementary school sites, one secondary school site, Low Density Residential uses, Medium Density Residential I uses, and Medium Density Residential II uses and collector roads G – J, M, K, W east of

Trafalgar Road, Louis St. Laurent Avenue east of Trafalgar Road and Part of collector road Q south of Britannia Road. Phase 1 includes approximately 42,950m² gross floor area of commercial space in Neighbourhood Centre Mixed Use II and Local Centre areas.

Phase 1 is further broken down into Stage 1, Nodes and Stage 2. Each are detailed in the following section.



Phase 1 Summary

Population
28,040

Units
10,720

Jobs
3,965

Commercial GFA
42,950 m²

P1 1

P2 6

VS 7

Elementary Schools 5

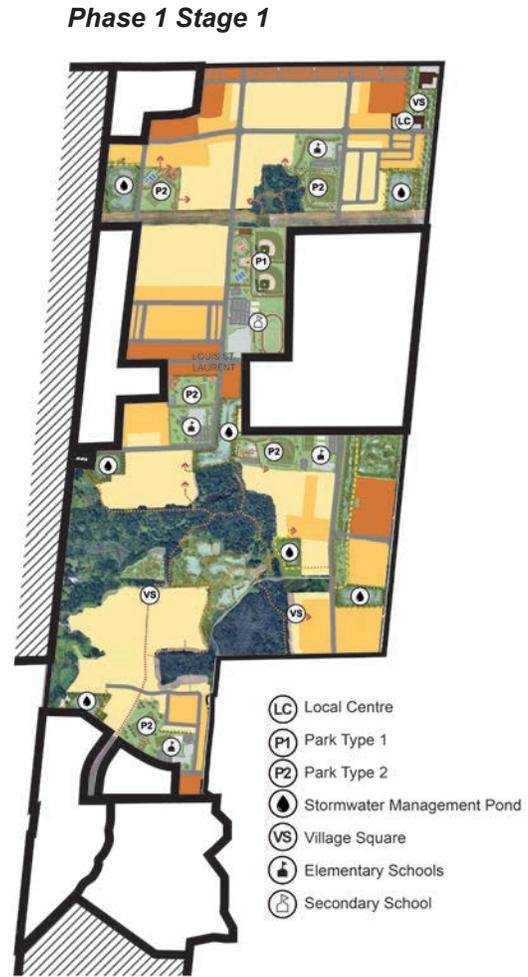
Secondary School 1

Stormwater Management Ponds 11

-  26km of local roads
-  15km of collector roads
-  **7km of MUPs**
-  **7km of on-boulevard bike lanes**
-  3km of public lanes
-  **8km of off-road trails**

Phase 1 Stage 1 Summary

Population 13,125	 19km of local roads
Units 3,955	 12.5km of collector roads
Jobs 1,635	 3km of MUPs
Commercial GFA 3,350m ²	 4km of on-boulevard bike lanes
P1 1	 2km of public lanes
P2 5	 7km of off-road trails
VS 3	 Singles 1,645 units
 Towns 1,750 units	 Back-to-Back 80 units
 4	 Stacked Towns 160 units
 1	 Apartments 320 units
 8	



4.2.1 Phase 1 Stage 1

Stage 1 lands will include densities to support transit and will be coordinated with the timing and availability of services from the Region. The construction of a range of housing types, based on allocation, will work to establish complete neighbourhoods.

The development of higher density housing, as well as a variety of housing types will achieve the targets of the Secondary Plan.

At Stage 1, there is sufficient allocation to satisfy Policy C.11.7.1.2.a). The proposed population allocated from participating

landowners equals approximately 13,125 meeting the required population of 8,000 people ensuring staging of development occurs in a manner that provides necessary services and is fiscally responsible.

This stage proposes to provide approximately 560 stacked townhouses, back-to-back townhouses and apartments to assist in meeting the affordable unit target of 30% of all new residential units within the Secondary Plan.

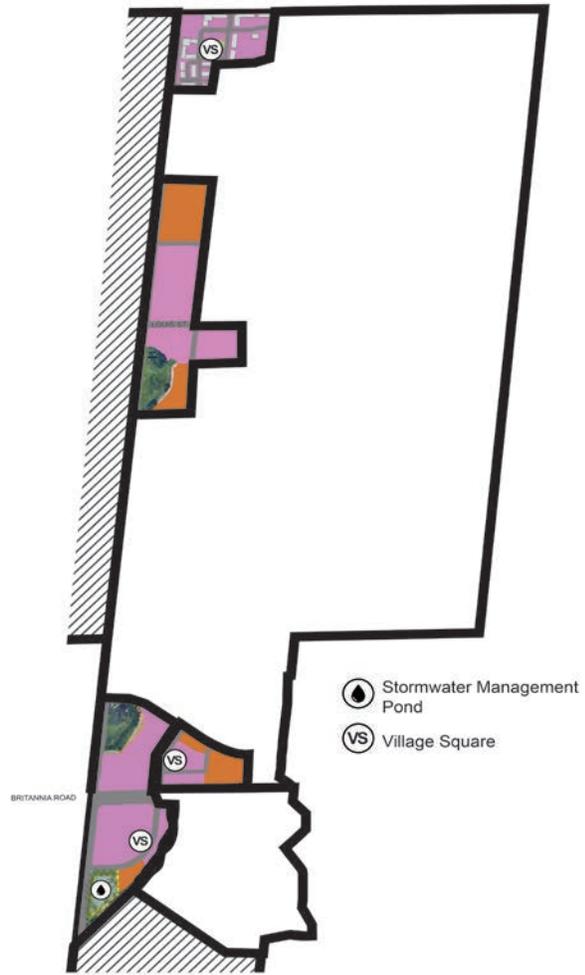
Within these lands the majority of community facilities will be delivered. This will include the school sites, parks, trails, MUPs, on road bike lanes, and collector roads.

4.2.2 Phase 1 Nodes

The nodes are key areas with a mix of uses and a mix of built forms that will contribute to the overall development of a complete community. These lands include medium and high density, mixed use and commercial lands. The development of the Nodes as transit supportive development will ensure ridership for the Transportation Corridor along Trafalgar Road.

These lands may proceed at the same time as the overall lands in Stage 1. Some landowners have purchased allocation for a portion of the units in the nodes through the 2023 Regional Allocation Program. These units will contribute to a mix of short term and long term development. The Nodes propose 5,065 units with approximately 95% of those meeting the criteria for affordable housing requirements of the Secondary Plan.

Phase 1 Nodes



Phase 1 Nodes Summary

Population

9,780

Units

5,065

Jobs

1,725

Commercial GFA

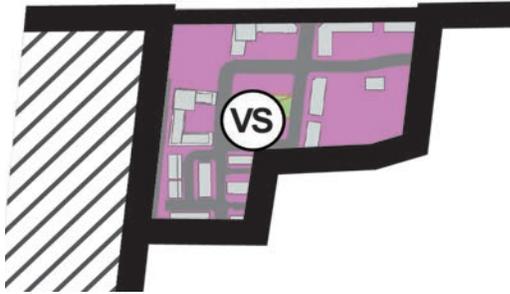
37,600 m²

VS 3

1

340m of off-road trails

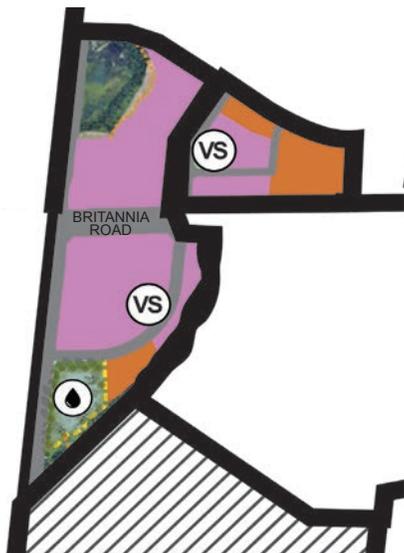
- Towns**
355 units
- Back-to-Back**
130 units
- Stacked Towns**
80 units
- Apartments**
4,500 units



NCMU II lands at the intersection of Trafalgar Road and Derry Road include medium density and high density mixed use built form. The NCMUII lands delivered in Phase 1 will contribute approximately 12,600 m² commercial GFA to the overall complete community providing in addition to the Local Centre services such as major commercial, personal services and retail, and park spaces through the inclusion of a Village Square.



A portion of lands south of the Gas Easement and east of Trafalgar Road will include medium density and high density mixed use built form. The NCMUII lands developed in this area will continue to contribute approximately 11,500 m² of commercial GFA to the overall complete community providing services such as major commercial, personal services and retail. These lands include a portion of Louis St. Laurent Avenue east of Trafalgar Road.



The portion of lands north and south of Britannia Road, east of Trafalgar Road. As with all the lands in Phase 1 Nodes, a mix of medium and high density mixed use built form, a stormwater management pond and two village squares are proposed. The NCMUII lands developed in this area will contribute approximately 13,500 m² of commercial GFA to complete this community by providing additional services such as major commercial, personal services and retail.

4.2.3 Phase 1 Stage 2

Currently, part of Stage 2 includes non-participating lands with the remainder of the lands adjacent to the Neighbourhood Centre at Trafalgar and Britannia Roads.

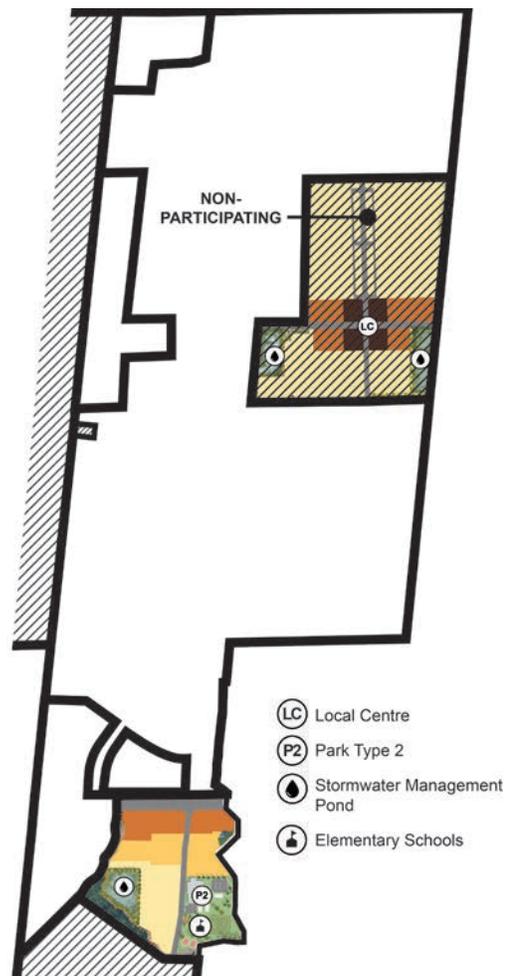
A mix of Low Density, Medium Density and Mixed Use lands are captured in this Stage.

Within these lands community facilities will be delivered. This will include a school site, parks, trails, MUPs, on road bike lands and collector roads.

This Stage includes an approximate population of 5,135. This stage includes approximately 1,700 eligible units to contribute to affordable housing.

The following describes the community facilities proposed to be delivered as part of Stage 2 that represent the participating landowners.

Phase 1 Stage 2



Phase 1 Stage 2 Summary

Population

5,135

Units

1,700

Jobs

605

Commercial GFA

2,000 m²

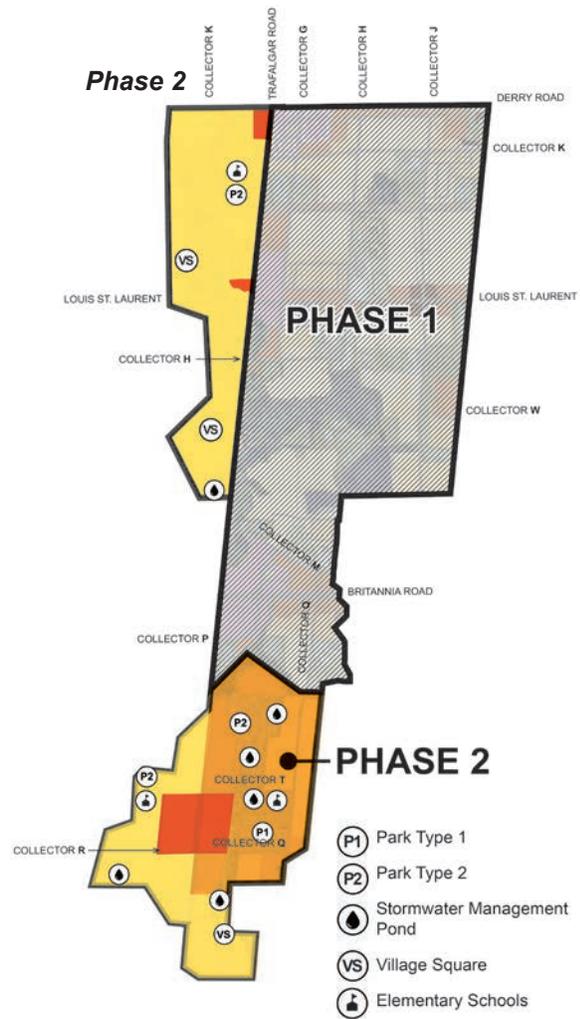
-  VS 1
-  P2 1
-  1
-  3

-  7km of local roads
-  2km of collector roads
-  2km of MUPs
-  500m of on-boulevard bike lanes
-  800m of public lanes
-  745m of off-road trails

-  Singles 720 units
-  Towns 360 units
-  Back-to-Back 30 units
-  Stacked Towns 130 units
-  Apartments 460 units

4.3 Phase 2

Phase 2 proposes the delivery of all the lands west of Trafalgar Road and a majority of lands south of Britannia Road. These lands will see the completion of all the Neighbourhoods and the creation of a complete community. Phase 2 includes the development of Neighbourhood Centre Mixed Use I and the completion of two of the NCMU II areas, a Park Type 1 (P1), three Park Type 2 (P2), six stormwater management ponds, three Village Squares, three elementary school sites, Low Density Residential uses, Medium Density Residential I and II uses, Neighbourhood Centre Mixed Use I and II uses, and collector roads T, K, Q and R and Louis St. Laurent.



Phase 2 Summary

Population	Units	Jobs	Commercial GFA
16,960	6,540	2,075	11,900 m ²
P1 1	8km of local roads		
P2 3	5km of collector roads		
VS 3	2km of MUPs		
3	5km of on-boulevard bike lanes		
8	380m of public lanes		
	6km of off-road trails		

Phase 2 includes 11,900 m² gross floor area of commercial space in Neighbourhood Centre Mixed Use I and II areas.

The Phase 2 population is estimated at 16,960 and 6,540 units are proposed. Of those units, 3,475 contribute to affordable housing.

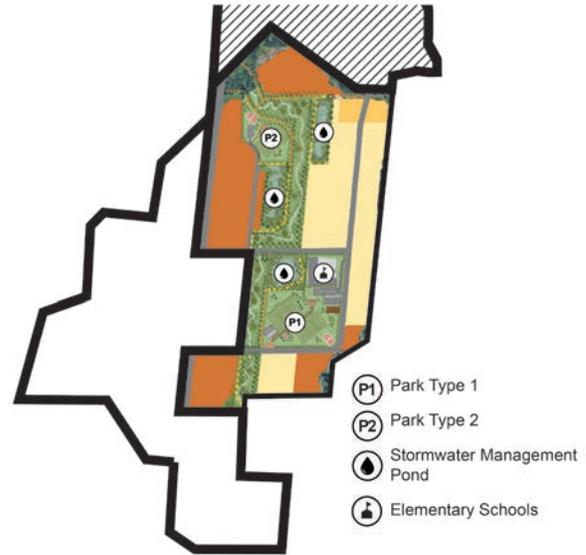
Phase 2 is further broken down into stages. Stage 1, Nodes, and Stage 2 are detailed in the following section.

4.3.1 Phase 2 Stage 1

Phase 2 Stage 1 includes a mix of Low Density and Medium Density lands. Within these lands some community facilities will be delivered. These lands provide a mix of low and medium density built form and include three stormwater management ponds, one Park Type 1, and one Park Type 2. These lands include the remainder of collector road Q and collector road T.

These lands propose 1,570 units with approximately 27% of those meeting the criteria for affordable housing requirements of the Secondary Plan.

Phase 2 Stage 1



Phase 2 Stage 1 Summary

<i>Population</i>	<i>Units</i>	<i>Jobs</i>
4,990	1,570	545

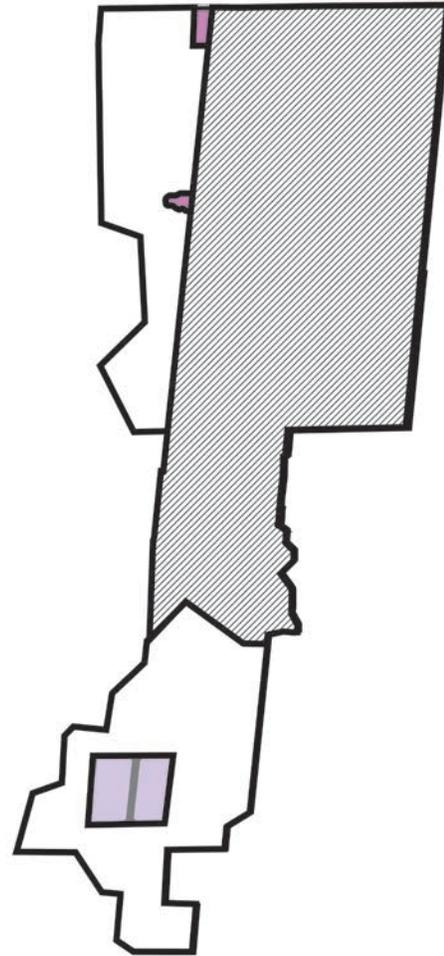
 1	 3km of local roads	 Singles 620 units
 1	 2km of collector roads	 Towns 530 units
 1	 500m of MUPs	 Back-to-Back 90 units
 3	 3km of on-boulevard bike lanes	 Stacked Towns 170 units
	 3km of off-road trails	 Apartments 160 units

4.3.2 Phase 2 Nodes

As with Phase 1, a minimum of 75% of the gross developable area of those lands participating in the landowner’s cost sharing agreement need draft plan or site plan approval before construction in the next stages can occur. This excludes lands designated Neighbourhood Centre Mixed Use I and II (Nodes). These lands may proceed at the same time as the overall lands in Phase 2 Stage 1. The development of the Nodes includes medium and high density, mixed use and commercial lands.

The completion of the Nodes will further contribute to the Transportation Corridor along Trafalgar Road and the creation of complete neighborhoods within this community.

Phase 2 Nodes



Phase 2 Nodes Summary

Population

6,180

Units

3,245

Jobs

860

Commercial GFA

11,900 m²



Towns

190 units



Back-to-Back

60 units



Stacked Towns

175 units



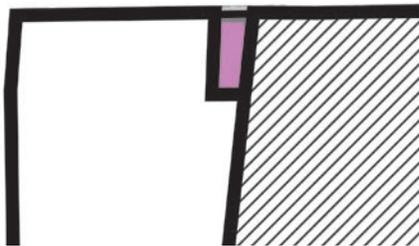
Apartments

2,820 units

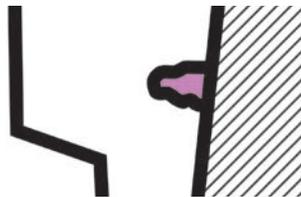


An example of a Village Square park in Milton

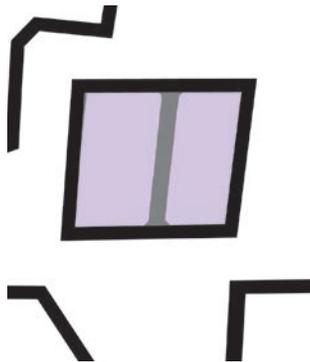
Phase 2 Nodes could provide approximately 95% of the units as affordable units and an approximate population of 6,180.



These lands will include medium density and high density mixed use built form. The NCMUII lands delivered in Phase 2 may contribute a small portion of commercial lands. These lands include a portion of collector road K.



These lands represent a portion of NCMUII lands west of Trafalgar Road.

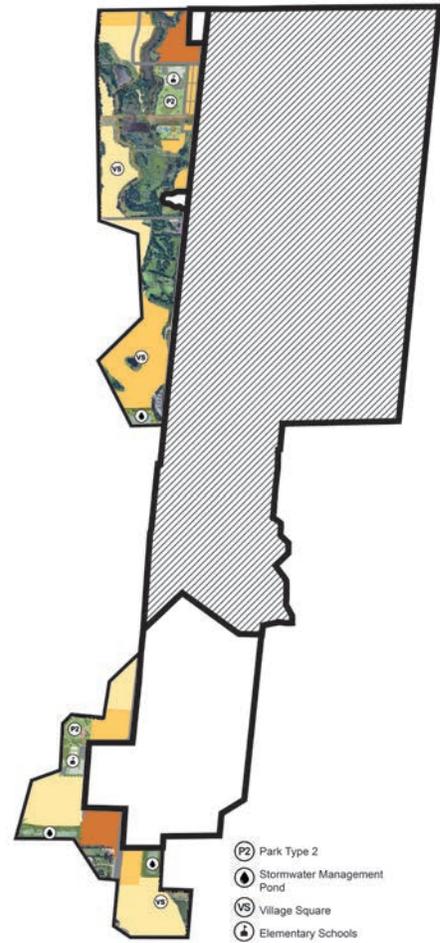


The NCMU I lands will contribute approximately 11,900 m² with a greater portion proposed for the east side of Trafalgar Road to the overall complete community, providing services such as major commercial, personal services and retail.

4.3.3 Phase 2 Stage 2

Stage 2 includes Low Density and Medium Density lands. This Stage also includes parks, trails, MUPs, on road bike lanes, and collector roads. The following describes the community facilities proposed to be delivered as part of Phase 2 Stage 2.

Phase 2 Stage 2



Phase 2 Stage 2 Summary

Population

5,790

Units

1,725

Jobs

670

-  **P2** 2

-  **VS** 3

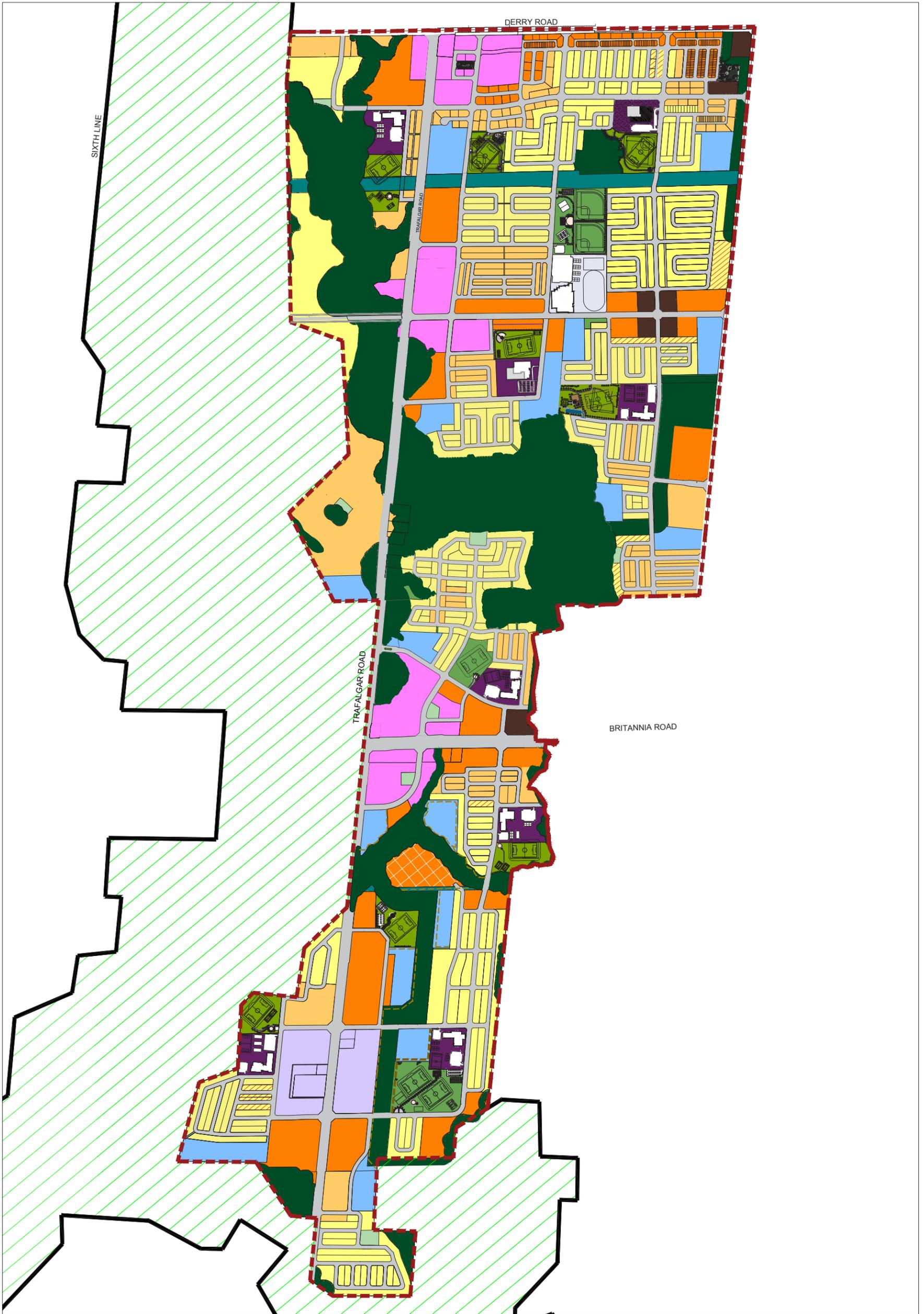
-  2

-  5

-  **5km of local roads**
-  **3km of collector roads**
-  **1.5km of MUPs**
-  **1.5km of on-boulevard bike lanes**
-  **380m of public lanes**
-  **3km of off-road trails**

-  **Singles**
670 units
-  **Towns**
1,055 units

APPENDIX A



MILTON PHASE 4
DEMONSTRATION PLAN
 June 2025

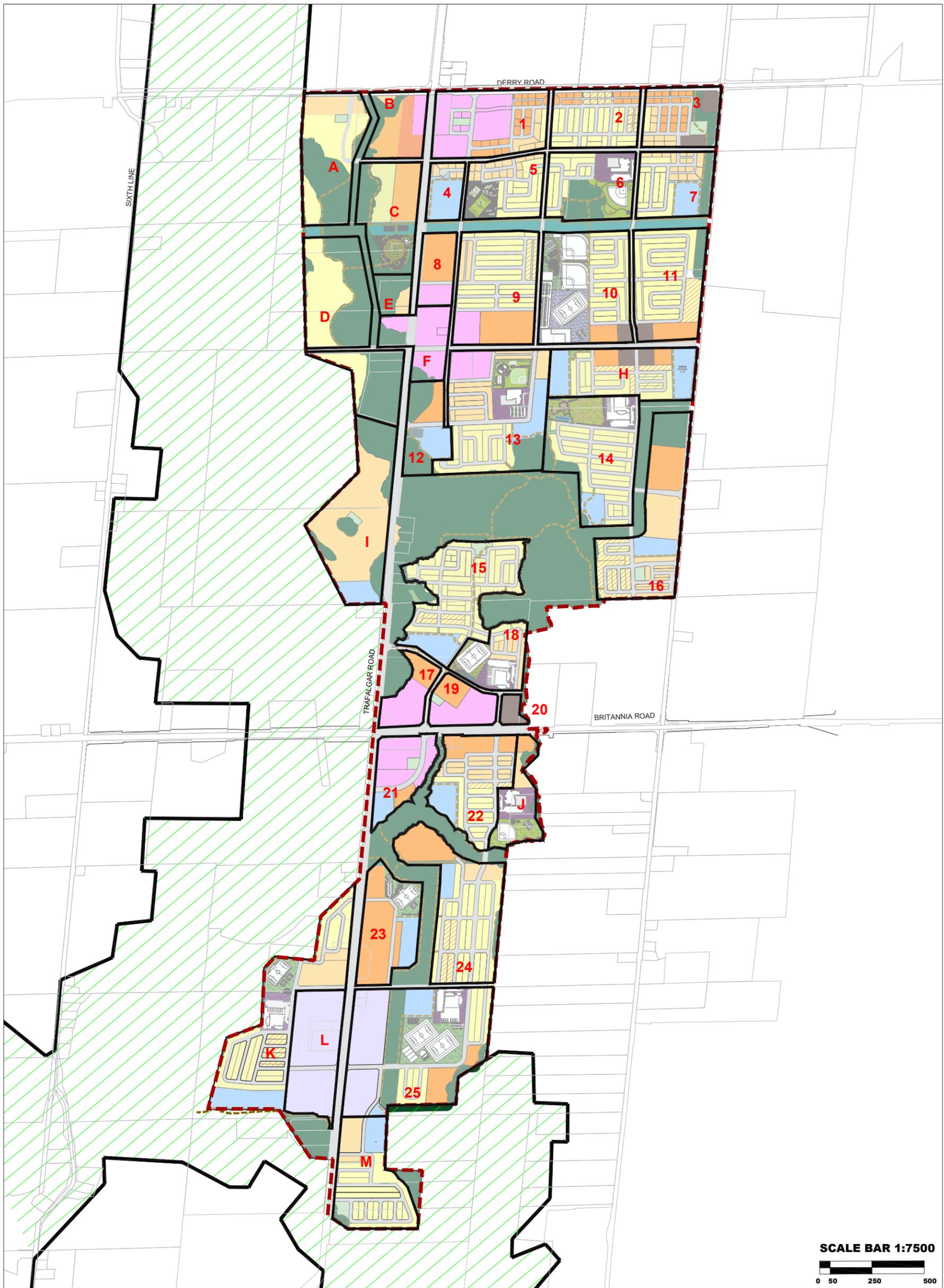
- | | | |
|--|-----------------------------------|----------------------------|
| Tertiary Plan Boundary | Low Density Residential | District Parks Type P1 |
| Conceptual Trail System | Low Density Townhouses | Neighbourhood Park Type P2 |
| Greenbelt Plan | Low Density B2B Townhouses | Village Square |
| Protected Country | Medium Density Residential I | Natural Heritage System |
| Protection for Potential Minor Arterial Road Extension | Medium Density Residential II | SWM Ponds |
| | Medium Density Residential II** | Roads |
| | Medium Density Mixed Use | Servicing |
| | Neighbourhood Centre Mixed Use I | Pipeline |
| | Neighbourhood Centre Mixed Use II | Elementary School |
| | | Secondary School |

*Trails within SWM facilities subject to location of maintenance access road.
 **Sites not along an Arterial or Collector road where apartments are permitted



Scale: 1:7500
 April 1, 2025

APPENDIX B



**TRAFALGAR
TRANSIT PLAN**

- | | | |
|--|-----------------------------------|----------------------------|
| Tertiary Plan Boundary | Low Density Residential | District Parks Type P1 |
| Conceptual Trail System | Low Density Townhouses | Neighbourhood Park Type P2 |
| Greenbelt Plan Protected Country | Low Density B2B Townhouses | Village Square |
| Protection for Potential Minor Arterial Road Extension | Medium Density Residential I | Natural Heritage System |
| | Medium Density Residential II | SWM Ponds |
| | Medium Density Mixed Use | Roads |
| | Neighbourhood Centre Mixed Use I | Pipeline |
| | Neighbourhood Centre Mixed Use II | Elementary School |
| | | Secondary School |

*Trails within SWM facilities subject to location of maintenance access road.

SCALE BAR 1:7500
0 50 250 500

Scale: 1:7500
September 15, 2023

APPENDIX C

Designation	Unit type mix	% Unit Mix	PPU By Unit Type	Density range By Unit Type
LD	Singles	89%	3.95	32
	Towns	11%	3.149	
MDI	Singles	0%		55
	Towns	100%	3.149	
MDII	Towns	26%	3.149	100
	Back2Backs	8%	3.149	
	Stacks	33%	1.801	
	Apartments	33%	1.801	
NCMUI	Stacks	30%	1.801	190
	Apartments	70%	1.801	
	Comm. GFA	12,000m2		
NCMUII	Stacks	10%	1.801	190
	Apartments	90%	1.801	
	Comm. GFA	36,000m2		
MDMU	Towns	33%	3.149	100
	Stacks/Back2Backs	33%	3.149	
	Apartments	33%	1.801	
	Comm. GFA	3977m2		

APPENDIX D

Conceptual /Estimated Commercial GFA for the Trafalgar Secondary Plan Area

	Secondary Plan Requirements	Commercial GFA Range m ² *	Commercial GFA provided in Tertiary Plan by Phase	GFA Location Description	Approximate GFA for each area identified in the Tertiary Plan
Phase 1, Stage 1	up to approximately 2,000 m ²	up to 2,000	3,350	MDMU south of Derry, east of 8th line (local centre)	1,850
	up to approximately 2,000 m ²	up to 2,000		MDMU north of Britannia	1,500
Phase 1, Node	each node between 9,000 m ² and 14,000 m ²	9,000 - 18,700	37,600	NCMUII at Derry	12,600
	total for phase 1 - between 27,000 m ² and 42,000 m ²	9,000-17,000		NCMUII at Louis	11,500
		9,000 - 19,000		NCMUII at Britannia	13,500
Phase 1, Stage 2	up to approximately 2,000 m ²	up to 2,000	2,000	MDMU on Louis St. Laurent (local centre)	2,000
Phase 2, Stage 1			0	0	0
Phase 2, Node	between 9,000 m ² and 14,000 m ²	9,000 - 17,000	11,900	NCMUI	11,900
Phase 2, Stage 2			0	0	0
Total			54,850		54,850

* Commercial Study recommended between 53,883 m² and 78,039 m² of commercial GFA

Appendix 2: Growth Projections for Trafalgar Secondary Plan Area (Cumulative)

	2025	2030	2035	2040	2045	2051
Population	-	8,786	24,756	38,718	45,180	45,180
Units by Type						
Single/Semi	-	1,069	2,366	3,354	3,655	3,655
Townhouse	-	1,408	2,805	4,155	4,630	4,630
Townhouse - High Density	-	24	269	593	715	715
Apartment	-	40	3,305	6,141	8,077	8,077
ADU	-	8	78	145	183	183
Total	-	2,549	8,823	14,387	17,260	17,260
Employment	-	358	2,879	4,943	5,815	6,040
Square Metres of ICI	-	7,600	61,040	104,790	123,290	128,050
Assessment Growth (Unweighted)						
Single/Semi	\$ -	\$ 249,347,866	\$ 1,128,355,937	\$ 1,873,822,689	\$ 2,216,088,635	\$ 2,216,088,635
Townhouse	-	233,514,950	1,122,348,534	1,696,790,695	2,136,707,940	2,136,707,940
Townhouse - High Density	-	-	47,275,938	188,817,807	255,919,138	272,599,156
Apartment	-	-	446,984,816	1,386,234,744	1,911,132,024	2,089,470,618
ADU	-	-	1,082,616	2,395,575	3,395,419	3,700,688
Subtotal	\$ -	\$ 482,862,816	\$ 2,746,047,840	\$ 5,148,061,510	\$ 6,523,243,156	\$ 6,718,567,037
Commercial	-	-	9,584,246	68,593,634	115,546,244	139,812,739
Institutional	-	-	2,367,131	6,248,373	7,805,135	7,805,135
Total	\$ -	\$ 482,862,816	\$ 2,757,999,217	\$ 5,222,903,517	\$ 6,646,594,534	\$ 6,866,184,911

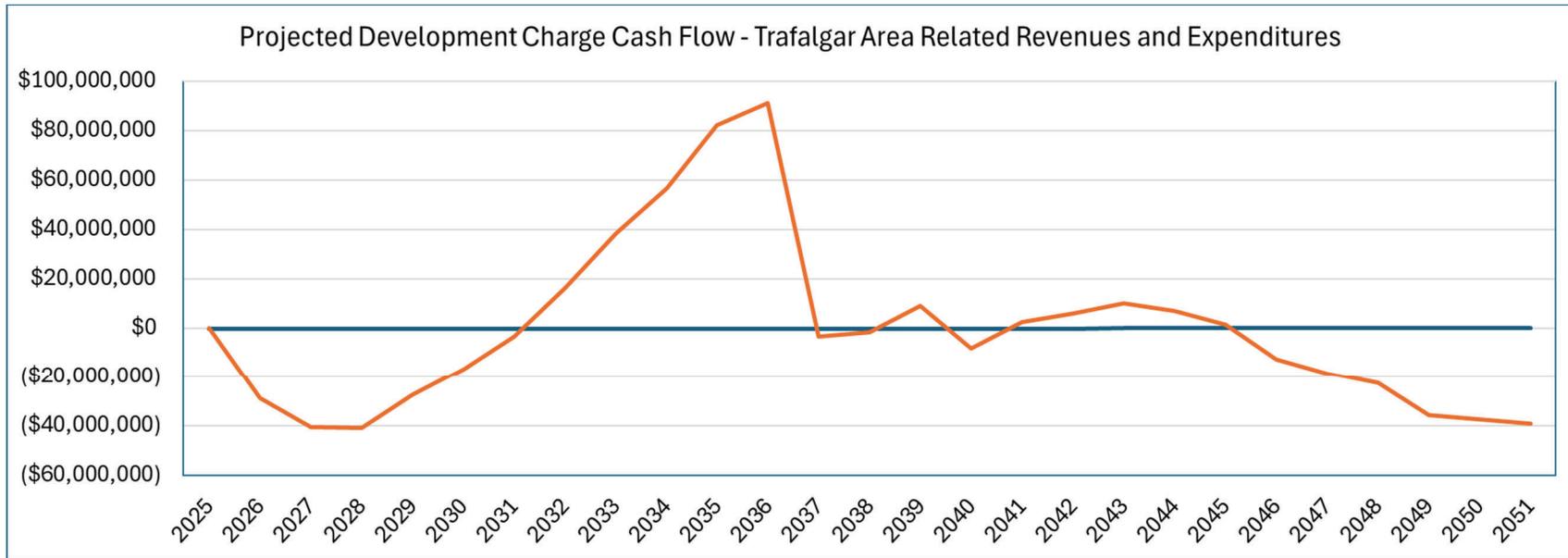
Note: Assessment growth presented with Current Value Assessment values as of January 1, 2016.

Appendix 3 - DS-070-25

Appendix 3: Infrastructure Value - Trafalgar Secondary Plan Area Portion of Constructed and Assumed Assets (\$2025)

	2025	2030	2035	2040	2045	2051
Assumed Assets						
Roads						
Collector Roads	\$ -	\$ -	\$ 30,998,828	\$ 59,903,481	\$ 76,608,730	\$ 81,365,584
Local Roads	-	-	40,907,201	86,109,280	114,862,526	122,510,982
Laneways	-	-	3,677,229	7,366,360	8,941,945	9,399,888
Structures	-	-	1,725,000	6,900,000	10,350,000	10,350,000
SWM Ponds	-	-	23,989,949	71,969,847	119,949,745	151,936,344
Subtotal	\$ -	\$ -	\$ 101,298,207	\$ 232,248,968	\$ 330,712,946	\$ 375,562,798
Constructed Assets						
Transportation	\$ -	\$ 13,714,694	\$ 24,565,573	\$ 39,981,254	\$ 54,915,748	\$ 74,901,598
Stormwater	-	-	-	-	-	-
Transit	-	20,130,588	23,607,293	26,146,289	27,396,033	27,396,033
Recreation	-	12,510,605	19,647,640	161,523,214	178,587,631	185,254,633
Library	-	37,275	37,275	18,969,889	18,969,889	18,969,889
Fire	-	8,174,492	8,272,826	9,691,600	11,037,506	12,951,631
Public Works / Operations	-	791,984	2,765,117	10,713,542	11,426,077	11,433,694
Other	-	2,566,926	2,566,926	2,566,926	2,566,926	2,566,926
Subtotal	\$ -	\$ 57,926,563	\$ 81,462,650	\$ 269,592,714	\$ 304,899,809	\$ 333,474,403
Total	\$ -	\$ 57,926,563	\$ 182,760,857	\$ 501,841,682	\$ 635,612,755	\$ 709,037,202

Appendix 4: Projected DC Cash Flow - Trafalgar Secondary Plan Area Related Revenues and Expenditures (\$2025)



Note: Based on existing development charge rates and service levels as of 2025.

Appendix 5 - DS-070-25

Appendix 5: Estimated Operating Budget Impact (\$2025)

	2025	2030	2035	2040	2045	2051
Gross Operating Expenditures	\$ -	\$ 5,074,136	\$ 10,576,388	\$ 17,834,558	\$ 21,146,595	\$ 23,125,787
Less: Non-Tax Revenues	-	(1,492,447)	(4,460,158)	(8,071,254)	(9,252,002)	(9,282,657)
Net Operating Expenditures	\$ -	\$ 3,581,689	\$ 6,116,230	\$ 9,763,304	\$ 11,894,593	\$ 13,843,130
Lifecycle Costing for New Infrastructure	\$ -	\$ 1,839,795	\$ 4,502,395	\$ 13,237,936	\$ 15,826,419	\$ 17,132,578
Transfer for DC Exemptions	-	311,234	311,234	311,234	311,234	311,234
General Tax Levy Requirement	\$ -	\$ 5,732,718	\$ 10,929,859	\$ 23,312,474	\$ 28,032,246	\$ 31,286,942
Tax Revenue (2025 Tax Rates)	-	(1,712,608)	(9,801,368)	(18,645,669)	(23,773,730)	(24,591,858)
(Surplus)/Shortfall	\$ -	\$ 4,020,109	\$ 1,128,491	\$ 4,666,806	\$ 4,258,517	\$ 6,695,084

Note: Based on existing property tax rates and service levels as of 2025, as well as continuation of the Town's existing financial policies with respect to the lifecycle funding for infrastructure.